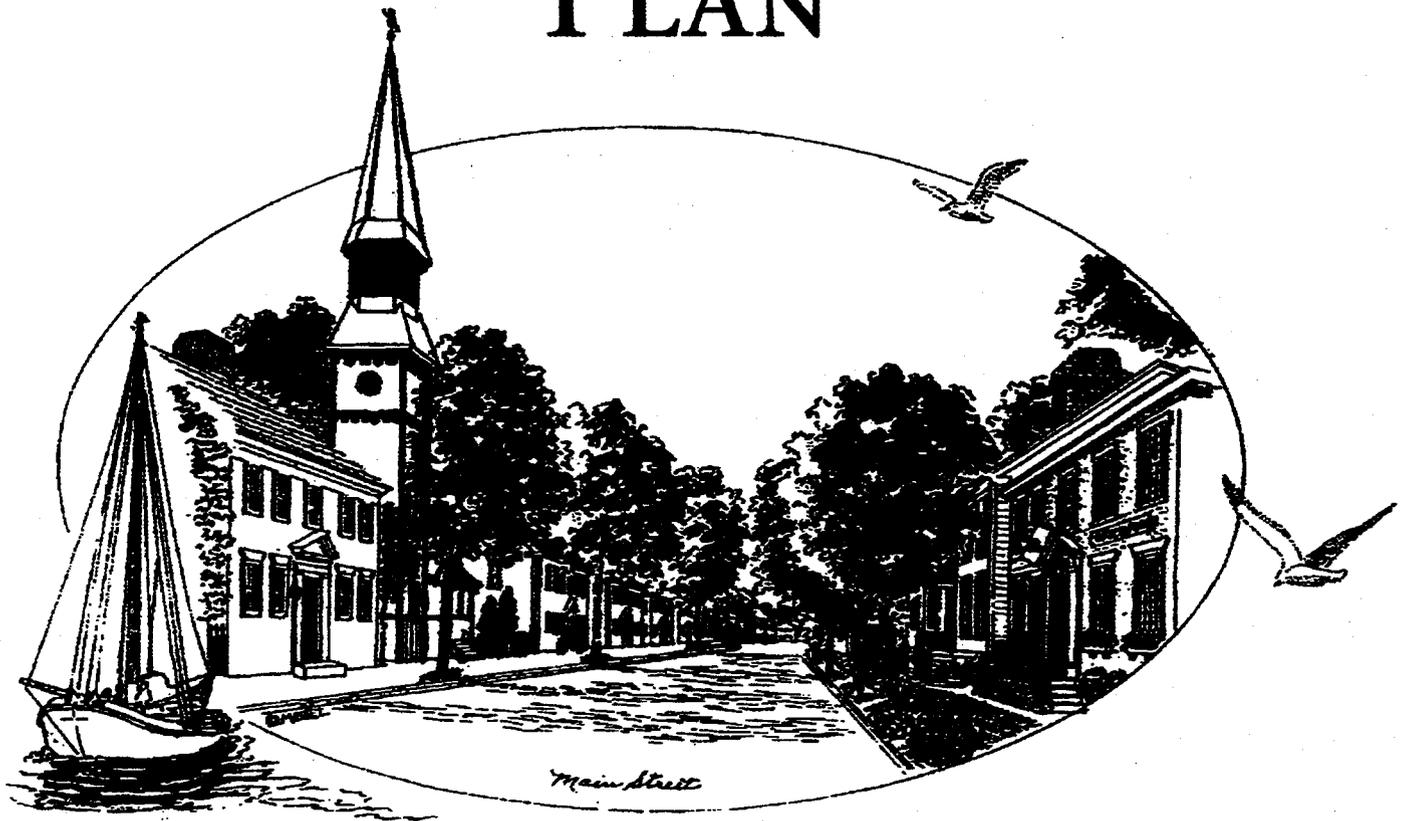


# WICKFORD VILLAGE PLAN



WICKFORD VILLAGE PLAN ADVISORY COMMITTEE  
*and*  
DEPARTMENT OF PLANNING AND DEVELOPMENT  
*North Kingstown, Rhode Island*

May 1998

PLAN DEPT  
OFFICE COPY

## **WICKFORD VILLAGE PLAN**

**April, 1998**

**Wickford Village Planning Advisory Committee  
and the  
Department of Planning & Development  
North Kingstown, Rhode Island**

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The preparation of the Wickford Village Plan and the Design Guidelines were funded in part by a grant from the National Trust for Historic Preservation through the Antoinette F. Downing Preservation Services Fund for Rhode Island.

## **VISION STATEMENT**

**The purpose of the Wickford Village Plan is to protect and enhance the traditional character of historic Wickford Village by preserving the village's sense of place as well as natural, historical, cultural and aesthetic values; to maintain Wickford Village as an attractive community with economic vitality; and to establish design standards as well as guidelines for public safety and mobility while improving village infrastructure.**

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  - Town Council Resolution No. 19 establishing the Wickford Village Plan Advisory Committee.**

## **PLANNING AREA GOALS**

**The Wickford Village Plan Advisory Committee adopted four goals for the planning area:**

**Goal No. 1 - Enhance the historic character and vitality of Wickford Village.**

**Goal No. 2 - Improve the Village's Infrastructure, traffic and pedestrian circulation, parking, and alternative modes of transportation.**

**Goal No. 3 - Explore and maintain the balance in the residential and business community.**

**Goal No. 4 - Anticipate, prepare for, and manage growth and change.**

## **EXECUTIVE SUMMARY**

The report of the Wickford Village Plan Committee has two major components: the Action Plan that follows and a set of village Design Guidelines prepared by consultant, ABACUS, Inc. Preparation of the Design Guidelines was funded with a matching grant from the National Trust for Historic Preservation.

The Action Plan sets the stage for the Design Guidelines. The richness of Wickford's history is described to enhance understanding of the need for a blueprint for the future. Issues and concerns raised by the Committee and through a citizen's survey are described. Based on the issues, goals were established around four themes that emerged from the issues discussion: sense of place, functionality, quality of life, and aesthetics. Citizens and business owners responding to the survey questionnaire noted that what they liked most about the village was the variety of services; the sense of community; the village charm; the harbor and water views; and pedestrian scale.

The Guidelines provide the Town, its boards and commissions, residents and businesses with guiding concepts for use in the development or redevelopment of structures or land in the village area. The Guidelines address both public and private buildings and sites. While the Guidelines focus primarily on the built environment, ideas about the need for walking trails, parks, and street trees are also presented. Finally, the Guidelines provide a framework for creating and sustaining the Town's vision of a village. The guidelines were developed for the village of Wickford, in particular, but many of the components and features displayed are appropriate for the Town's other villages.

One component of the design guidelines prepared by ABACUS is a set of intersection improvement plans for three key intersections: Brown Street and Phillips Street; Brown Street and West Main Street; and Phillips Street and Tower Hill Road. The Committee believes that accomplishing these improvements is important to the overall implementation of the Plan and improving the pedestrian nature of Wickford. The Committee understands that these improvements must be accomplished in concert with the State Department of Transportation.

The Design Guidelines were intended to be, in fact, guidelines. However, there may be some guidelines that the Town may choose to formally adopt either within the Village Design Guidelines of the zoning ordinance or, in some cases, as a dimensional standard in the ordinance.

The National Trust for Historic Preservation has developed a model "Main Street" Program which offers useful guidelines for Wickford's future. The Main Street Program focuses on four points: design, organization, promotion, and economic development.

Many of the elements of the program have already been applied in Wickford, but have

not yet been formalized as a management approach.

Four primary goals were developed for the Plan; each goal is presented with a set of objectives, strategies and actions. The four goals address the issues of enhancing the historic character and vitality of the Village; improving Village infrastructure such as streets, sidewalks, the proposed development of a boardwalk along the water, and intersection improvements; the creation of an advisory committee to foster communication among residents, the business community, and the Town; and managing future growth such that it continues to support the Village.

The Plan identifies improving Brown Street as its highest priority. There are several categories of improvements suggested. Infrastructure improvements such as sidewalk improvements, the narrowing of key intersections for pedestrian enhancements, the replacement of existing lighting fixtures, linking and enhancing the public green spaces such as Updike and Library Parks; and adding to public spaces by the development of a boardwalk along the water are key elements of the Plan vision. Undergrounding utilities during a future public works project is also considered an important infrastructure improvement. Some components of the Plan require no action by the Town; most notable of these recommendations is the importance to the village of keeping the Town Hall Annex as a municipal building.

Programs that support the business community in Wickford are a part of the Brown Street improvement program. The Plan recommends tax incentives to encourage façade and sign improvements and the development of directory signage for business establishments and to Town facilities, such as the restrooms, that assists visitors to Wickford.

Finally, it is the sense of place and pedestrian scale and the character of the village that attracts visitors and residents to Wickford village. The Plan recommends supporting these attributes by developing a means of protecting the character of Brown Street in a manner comparable to that afforded West Main and Main Street. Implementing this recommendation could take a variety of forms including the extension of the local historic district, or a variant, to include Brown Street or perhaps, the development of zoning guidelines that assure that not only scale but design are considered in the review of development or redevelopment proposals.

# **WICKFORD VILLAGE PLAN**

## **PART 1.0 INTRODUCTION**

The Town of North Kingstown is a community with a diverse physiography and a variety of settlement patterns ranging from Slocum's rich agricultural plain to the Pettaquamscutt estuary and from the large industrial park at Quonset Point / Davisville, fashioned from former U.S. Naval facilities, to the scattered villages. At the center of the Town is historic Wickford Village, which dates from Rhode island's earliest period. In August, 1996, the Town applied to the National Trust for Historic Preservation for a grant from the Antoinette F. Downing Preservation Services Fund for local preservation efforts. The National Trust responded in September, 1996, with approval of a \$1,000 matching grant for the preparation of a plan for Wickford Village with design guidelines.

On November 18, 1996, the North Kingstown Town Council adopted Resolution No. 19 which established the Wickford Village Plan Advisory Committee to prepare a plan for Wickford Village to "preserve and enhance the traditional character of Wickford Village, its sense of place and its economic vitality." The committee was charged with the responsibility of developing guidelines for public improvements and design guidelines for lighting, utilities, parking, street trees, pedestrian movement, vehicular circulation and access to local businesses, the waterfront, and to public spaces.

When commenting on the protection of Wickford's "sense of place", we are in fact referring to the protection of an idea that is characterized by elements which are uniquely manifested by the Village of Wickford. Wickford is a 300-year old village that is characterized by all the major attributes that are essential to a successful community: a well-developed village center that combines residential and business uses; interesting, coherent, and historically significant architecture; and a scale and ambiance that promote extensive pedestrian use.

### **1.1 Village Plan and Town Comprehensive Plan**

The North Kingstown Comprehensive Plan was completed in 1992, pursuant to Chapter 45-22.2 of the Rhode Island General Laws. It was adopted by the Town in August, 1992, and approved by the State in February, 1995. Preparation of the Comprehensive Plan was guided by a Citizen's Advisory Committee, similar to the committee created for the preparation of this Village Plan. The Comprehensive Plan recommended that the Town thoroughly analyze the Town's villages, determine their unique characteristics and qualities, including historic, natural, and cultural resources, and to prepare plans to ensure that village character is preserved in North Kingstown. This Village Plan will become a critical component of the Town Comprehensive Plan upon adoption by the Planning Commission and the Town Council.

## **1.2 Methodology**

Following creation of the Village Plan Advisory Committee, the Town Council advertised in the local Standard Times newspaper for citizen volunteers to serve on the Committee. Membership on the committee was open to all residents of North Kingstown, and a total of 23 volunteers were appointed to serve. The committee included Wickford Village residents as well as residents from other sections of town. It also included members of the Town Planning, the Historic District, Conservation and Harbor Management Commissions, as well as representatives of Historic Wickford, Inc. a residents group, the Chamber of Commerce, and the Wickford Merchants Association.

Thus, the committee possessed a wide range of design, business, management, and other skills. The Committee first met on January 23, 1997 and regularly thereafter to prepare a plan. Meetings generally were conducted twice monthly at the Cold Spring Community Center on Beach Street. Meetings were posted as required by law. The committee elected two members to serve as co-chair persons. Committee members signed an attendance form at each meeting, and meeting minutes were maintained for each meeting. Subcommittees were formed to address the issues related to: (1) waterfront and maritime concerns; (2) village infrastructure; and (3) protecting historic character and managing growth.

The Committee began by defining the boundaries of the planning area, defining issues associated with the village, and setting goals. The Committee conducted a walking tour of the village on May 22, 1997, to assess its physical characteristics, to identify problems, and to suggest improvements. On June 5, 1997, the committee met with Wickford Merchants and reviewed with them slide photographs taken by Planning Department staff during the walking tour and discussed topics and themes that village merchants have identified to improve the village. It was discovered at that meeting that the merchants and the committee shared many of the same ideas and concerns addressed in this report.

To gain an understanding of public sentiment on Wickford Village, the Committee developed a survey questionnaire for residents. The questionnaire was distributed town-wide in the "Compass," a newsletter about municipal affairs, through the North Kingstown Villager, a monthly publication mailed to all households. More than 200 residents responded to the survey. Nearly 87 percent of the respondents said Wickford Village was a positive factor in their choice to live in North Kingstown. An even larger number, 95 percent, said that the historic character of the village should be protected and enhanced. Ninety percent supported regulations to maintain the Village's historic character. A copy of the questionnaire and the survey results is provided in the Appendix.

The committee decided to secure a design consultant to assist with the development of Design Guidelines. A Request for Qualifications (RFQ) was prepared and advertised in the Providence Journal. A list of design firms, maintained by the Town's division of purchasing was notified of the RFQ. Four firms responded and made presentations to the Committee. The Committee selected ABACUS Architects & Planners of Boston and Wareham, MA. ABACUS met with the Committee and with the Wickford Merchants Association and Historic Wickford, Inc. to discuss issues and ideas pertaining to the Village and the preservation of Village character. Drafts of the design guidelines were submitted for review by the Committee in December, 1997 and January, 1998.

Following completion of the village plan and the design guidelines, and adoption by the Plan Advisory Committee, the plan was presented to the Planning Commission and the Town Council for review and adoption.

## **Part 2.0 DESCRIPTION OF STUDY AREA**

Wickford Village is located along the western shore of Narragansett Bay, south of Quonset Point. The village is one of nine historic villages in the Town of North Kingstown and the only one with a sheltered harbor (figure 1). Principle transportation routes to the village are Post Road (Route 1) from the north and Boston Neck Road (Route 1A) from the south. Route 102 provides direct access from the west to the village, from Route 4 the major limited access highway servicing the region, and further west, Interstate I-95. Tower Hill Road (Route 1) provides direct southerly access. Future development plans by the State Department of Transportation include improvements to Routes 4 and 1. There is also to be a new limited-access highway connection between Route 4 and the developing industrial park and port at Quonset Point/Davisville. Both projects will improve access to North Kingstown. The prospective development at Quonset Point/Davisville will continue to increase development pressures on the Town.

Wickford village is served by the municipal water system. The village, similar to most of the Town, is not serviced by a central sewage collection system. All sanitary waste is disposed in on-site septic systems. The Town plans to develop a wastewater management program to assure the proper operation and maintenance of on-site systems. Lack of an adequate wastewater management plan has been identified as a contributor to failing harbor water quality. The wastewater management program will encourage introduction of innovative septic systems to improve the quality of effluent discharged to the groundwater.

A significant historical area, Wickford Village is defined by a local Historic Zoning District and a National Register Historic District (figure 2). Of the two, the local Zoning

District encompasses the smaller geographic area. The local District does not include the Brown Street business district in the heart of the village. The larger National Register District does not extend to historic Smith's Castle. Smith's Castle is listed as an individual property on the National Register of Historic Places. The entire Cocumscussoc site is recognized as a National Historic Landmark. The National Register Historic District will be resurveyed in 1998 to evaluate the historical resources of the district and its immediate environs. It is anticipated that an application will be prepared in 1999 for the purpose of increasing the National Register Historic District boundaries.

In defining the boundaries of the village, the Committee noted that the Village could be defined in a number of ways: i.e. the local historic district and the national district. The Committee also recognized that the Village could also be defined by characteristics such as the distance covered in a fifteen-minute walk.

The Village Committee understood that the history and the historic character of Wickford Village would play an important role in the committee's work, but selected a wider definition of the village's boundaries, encompassing more of the waterfront and the highway approaches to the Village. The Committee recognized that the waterfront is integral to the historic development and the present economy of the Village as well as to the Town. The highway approaches serve as "gateways" and transition zones into the Village whether it be the destination or a location along the route to the south shore, Conanicut Island, or Newport.

The agreed village planning area encompasses a total land and water area of approximately 750 acres. In addition to those areas traditionally considered to be a part of Wickford village, the Committee's planning area incorporates the Wickford State Police Barracks, Smith's Castle, the Town Beach, the community center, the senior center, and the Poplar Point neighborhood, none of which are presently included in the historic district (figure 3).

## **2.1 Historical Background**

North Kingstown's human settlements probably date back 10,000 years before the grant to Roger Williams of land by the Narragansett Indian in 1636. Over a period of approximately 2100 years the Narragansett Indian culture had evolved into permanent settlements along the coastal plain and the fertile inland terraces, based on agriculture, fishing, and hunting. In 1637, Roger Williams established a temporary trading post in the vicinity of Cocumscussoc, convenient to land and water transportation, close to Indian villages and near the home of the friendly Indian Chief Canonicus. In 1643 he built a permanent home which he sold to his friend Richard Smith in 1651. Smith's Castle, as it came to be known, was burned after the Great Swamp Fight in 1675 and rebuilt around 1678. It became the political, social, and religious capital of the developing region.

Lodowick Updike laid out a road to the salt water for the purposes of creating a port. The road led down from the "Pequot Trail- Post Road" to the town he called Updike's Newtown. By 1709 the town was platted and lots were being sold along Pleasant Street and present day Main Street. The Old Yellow, which was built during this period in 1735, is the oldest surviving dwelling in Wickford. It was recently repaired and renovated through the inspiration and hard work of the Old Yellow Corporation, a group comprised of Wickford Village residents and others. All repairs were conducted under the auspices of the Rhode Island Historical Preservation and Heritage Commission.

Wickford, as Updike's Newtown came to be known, grew slowly and by the time of the Revolution there had been a history of land speculation involving unbuilt lots. Preservation Report W-NK-1, published by the Rhode Island Historical Preservation and Heritage Commission in November 1979, estimates that there were probably twenty standing houses by the time of the American Revolution. Twenty pre-Revolution houses and inns remain today and six date to before 1737.

Following the Revolution, Wickford entered a sustained period of growth. Boat building and shipping prospered. Agricultural products from inland regions were brought to Wickford for shipment to other towns and regions. As the Village's economy grew, so did local cultural, religious, and governmental institutions. But, by the third decade of the 19th century, the Village was eclipsed by other ports, as rail transport became increasingly more important for shipping. Wickford continued as a secondary port and a mill village. Fishing was an important economic activity and has remained so to the present, with numerous shell and lobster fishing boats operating from the town wharf.

As Wickford Village moved into the twentieth century, it became an important summer recreation colony. In 1932, the Main Street Association, formed as a civic improvement organization began its work. The association sought paved sidewalks, street trees, inaugurated a program of historic house markers and worked for the first zoning ordinance. In 1937, Colonel Hunter White published "Wickford and its Old Houses," the basis for the Wickford Historic District in 1959. This second Historic District formed in Rhode Island, led to the preservation Wickford and its fine old buildings, a fate not always the experience of other New England villages. Wickford survived the 1938 Hurricane, but the salt water contamination of local wells resulted in the construction of a town owned water distribution system. Following World War II, Wickford harbor became an important yacht basin for recreational boats and continues so today.

## **2.2 Overview of Existing Conditions**

Wickford village is a thriving example of a well-preserved colonial era village that

has evolved over a period of more than 350 years. Brown Street, the center of commercial activity, is complimented by stores and specialty shops on Main Street, West Main Street and Phillips Street. On these streets and others within the planning area, the village has food markets, a hardware store, a pharmacy, clothing and apparel stores, restaurants and delicatessens, antique and gift shops, art galleries, a book store, business office space, professional design offices, craftsmen and artisans, medical professionals and law offices, beauty shops, a liquor store, municipal offices and parking, a municipal dock and town wharf, a commercial fishing fleet that "works" Narragansett Bay, recreational boating facilities, marine repair shops and craftsmen, public recreational facilities and parks, a bird sanctuary, the town beach, the senior center, the community center, a band stand, the public library, an elementary school, a middle school, police, fire and rescue services, florists, a funeral home, automotive services, real estate sales offices, and several places of worship. More than 50 local businesses belong to the Wickford Merchant's association. The village is home to the popular Wickford Art Festival, now in its fourth decade and a number of seasonal events such as the festival of lights, the tuba concert, and the summer concert series.

The residential population of Wickford and its environs, according to the 1990 census, is somewhat less than 2,900 persons. The Village does not now experience a significant influx of seasonal residents.

Access to the Village is by highway from Post Road, Boston Neck Road, Tower Hill Road, and Ten Rod Road, all state highways; major improvements recommended by this plan that involve these roads recognize the involvement of the state in accomplishing such improvements. There is waterfront access for a limited amount of transient boaters. The state Department of Transportation is evaluating the feasibility of establishing a bay commuter service as a demonstration project. The service may connect at Quonset Point rather than at Wickford. Formerly, rail access to Wickford via the Seaview Railroad provided service along the shore route, and the Newport and Wickford Railroad extended from Wickford Junction through the village to Poplar Point. However, portions of the Seaview right-of-way remain and the Wickford Branch right-of-way is largely intact, for possible use as biking trails.

Wickford Harbor has two dredged federal channels, one in Mill Cove and one in Wickford Cove, and substantial boat mooring space to complement dock space. Moorings are managed by the Town's Harbor Master who is an employee of the Police Department. The harbor entrance is protected by a stone rubble breakwater. This breakwater is in need of repair. Even modest storm surge and very high tides overtop the structure. Maintenance of the federal project areas is the responsibility of the U.S. Army Corps of Engineers. The U.S. Coast Guard breaks the channel ice during winter freezes.

The Rhode Island Department of Environmental Management, in it's August 6, 1997 Water Quality Classifications, has classified the outer harbor's water quality as SA

(b) and Mill Cove and the inner harbor as SB. The SA(b) classification defines waters that support extensive recreational boating and are otherwise designated for shellfish harvesting for direct human consumption, primary and secondary contact recreational activities, and fish and wildlife habitat. They are suitable for aquacultural uses, navigation and industrial cooling, and have good aesthetic value. The SB classification defines waters that are designated for primary and secondary contact recreational activities, shellfish harvesting for controlled relay and depuration, and fish and wildlife habitat. These waters are considered to likewise be suitable for aquaculture, navigation, and industrial cooling, and have good aesthetic value.

The Rhode Island Coastal Resources Management Program, as amended, classifies the water uses for the inner Wickford Harbor and an area along the southern shore of Mill Cove (Cedar Tree Point to Pleasant Street) as a commercial and recreational harbor, the outer harbor as a high-intensity boating area, Fishing Cove as a low-intensity use area, and Mill Creek as a conservation area. These classifications reflect existing conditions.

The local economy in Wickford and North Kingstown is strong. In fact, there has been continuous public and private investment in Wickford in recent years. The Town has made major improvements to the Town Hall and to the Town Hall Annex over the past two years which has greatly improved the appearance and physical condition of these facilities. A major repair to the Town Wharf at the end of Main Street began in late 1997. The Town Dock off Brown Street behind the Town Hall Annex is a favorite for visitor short term transient docking. Several local streets have received pavement overlays and Loop Drive was improved with the extension of municipal water service. New culverts beneath that road maintain tidal flow between the inner and outer coves. Future public and public/private partnership investments are likely in public spaces, public buildings, and infrastructure.

Recent commercial investment in the Village has included: the painting of building facades, the opening of new businesses where none existed and the replacement of businesses that have relocated, the repair of antiquated on-site sewage disposal systems, new roofs on several buildings, the opening of a new 62 slip commercial marina to replace a former shellfish business, and the redevelopment of a commercial fin fish and shellfish wholesale business. Other investments include the rehabilitation of one of the oldest homes in the village; the repainting, rehabilitation or remodeling of numerous residential dwellings throughout the village; the construction of new residential docking facilities; and new homes in an architectural style that fits into the neighborhood. The rehabilitation of Updike Park, prominent in the village center, was accomplished recently through the efforts of private citizens and the municipal government, coordinated by the Updike Park Citizens Committee.

Future investment in the village by the private sector is likely to continue in the form of building improvements of all types. A recent change was the relocation of the

Village post office to a larger building, modern facility just north of the planning area on Post Road. The building that formerly housed the post office is expected to be reused, but the loss of Postal Service near the village center is regretted by many. At its new location, the Post Office regional facility, contributes to the revitalization of Post Road.

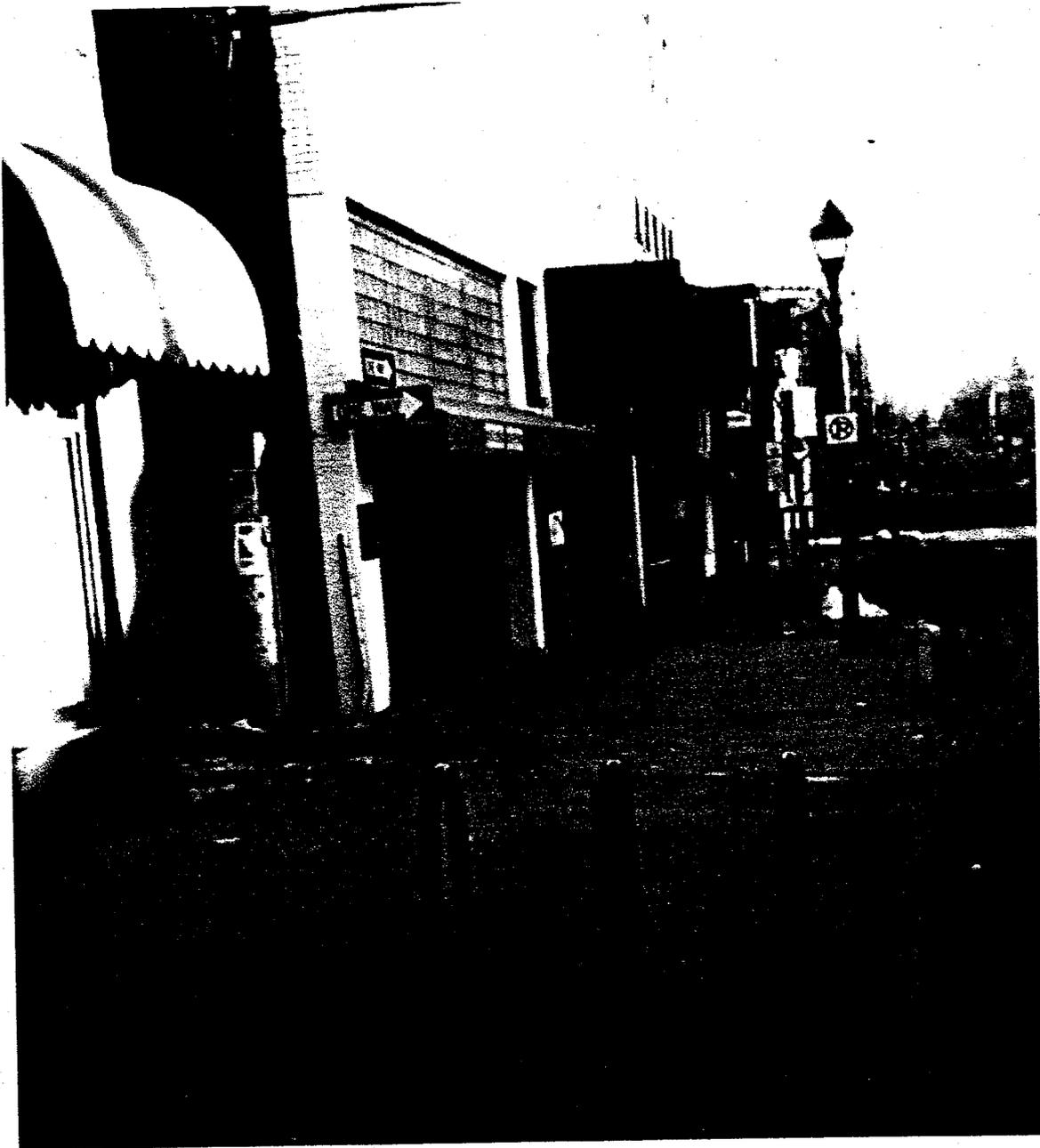
## **2.3 Issue Analysis**

The issues most frequently voiced by the Wickford Village Planning Advisory Committee can be summarized into the following categories: (1) sense of place, (2) functionality, (3) aesthetics, and (4) quality of life.

Sense of place as defined by the Village of Wickford refers to its ability to thrive as a "working place" where people live, do business, visit, and where there continues to be extensive investment in the community by both residents and businesses. Contributing also to the sense of place associated with Wickford is the placement of residences and businesses which provide the services essential for a given quality of life. The unique placement of these uses provides landmarks by which residents and visitors alike orient themselves to the "place" we call Wickford. Participation in the living, shopping and everyday chores are also contributing elements that define ones life and surroundings. Efforts must be made to ensure that there is a balance between changes to infrastructure as well as residential and commercial land uses which are unavoidable as one progresses into the future. The successful juxtaposition of 20<sup>th</sup>-century life, and 17<sup>th</sup> through 19<sup>th</sup>-century history has created a truly unique "sense of place" in Wickford that should be carefully preserved and enhanced.

Functionality refers to the working interrelationships of the various parts of the village: good traffic circulation, safe roads, proper and effective signage, good sidewalk conditions, trash receptacles, parking, lighting, drainage, public restrooms, and postal service. It also includes the usefulness of public spaces, such as the Town wharf and the municipal parking lot. It would include sewers if the town should install sewers and road improvements if the state Department of Transportation and the town decide on investing in them. Good functionality will be a guiding principle in the design of such improvements. Functionality may or may not typically include good access, but for the purposes of this plan, good access is considered integral with good function, and access may be by various modes of transportation. All but air and rail transportation should be considered, and even air transport via helicopter cannot be discounted for special purposes. The Committee believes that pedestrian and bicycle access and continued wharf or dock access is crucial to the ongoing functionality of the Village.

purposes of this plan, good access is considered integral with good function, and access may be by various modes of transportation. All but air and rail transportation should be considered, and even air transport via helicopter cannot be discounted for special purposes. The Committee believes that pedestrian and bicycle access and continued wharf or dock access is crucial to the ongoing functionality of the Village.



Providing bike racks, similar to this one in Flagstaff, Arizona, supports bicycling as a means of transportation.

pedestrians; 58 accidents with 14 injured, one a pedestrian, on West Main Street; 18 accidents with 3 injured, one a pedestrian, on Main Street; and 60 accidents with 29 injured, no pedestrians, on Phillips Street.

The Village Committee discussed frequently methods of traffic calming through the village, including routing unnecessary truck and school bus traffic around the village center. It was noted that a continuing factor to the traffic problem, from the perspective of the pedestrian, is the generous width of the intersections at both ends of Brown Street. At the north end, the distance from curb to curb is nearly 66-feet with no pedestrian refuge provided. At the south end, the curb to curb distance is 51-feet. The Committee favored closing up the width of the crosswalks at these locations to a more pedestrian friendly dimension.

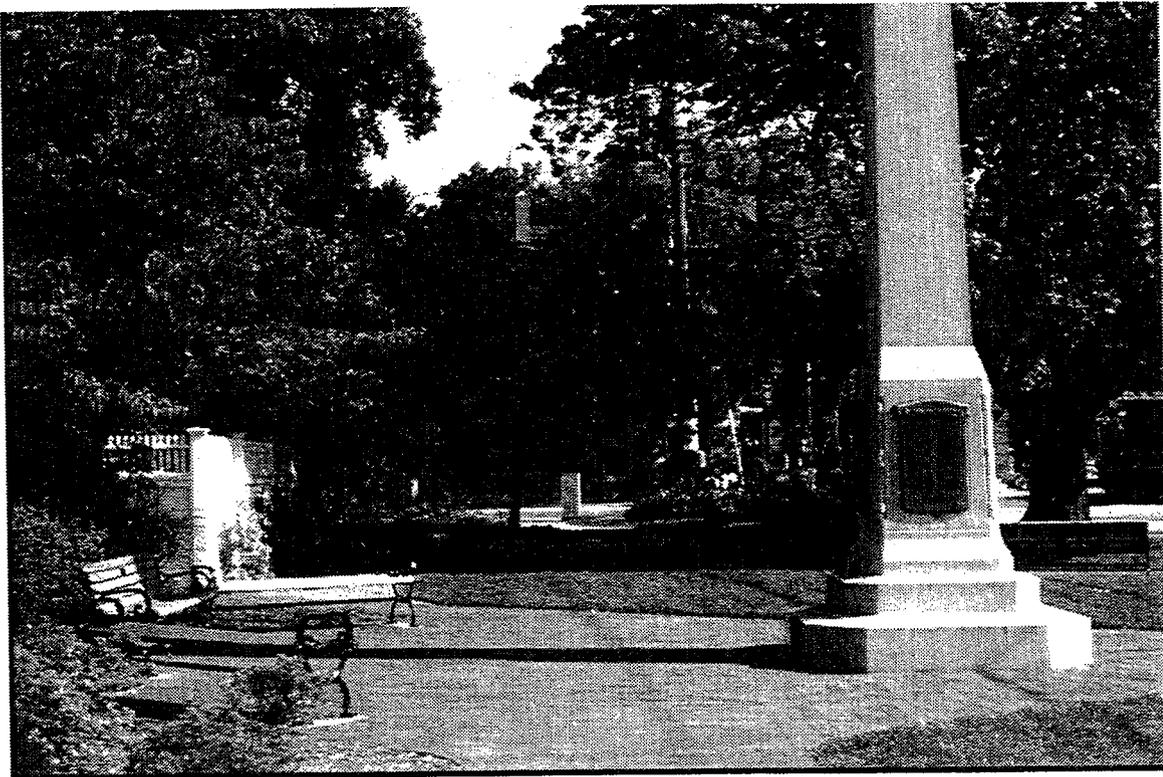


This bricked "bumpout" in Ithaca, New York narrows the distance between curbs, serves to calm traffic and provides a place for seating.

The Department of Planning and Development prepared a Village parking inventory. The total number of on-street and off-street spaces was compared to the

parking requirements for each land use in the village. This analysis shows 75,424 square feet of existing retail space, 26,725 square feet of office space, 19,571 square feet of warehouse space, and a restaurant seating capacity of 166 seats, 6008 square feet of banking space and 2,137 square feet for an auto service garage. Total Village parking available is 367 spaces. The total required by zoning for all of these activities is 601 spaces, a net parking deficit of 234 spaces. However, in the discussions of the Plan Advisory Committee, the parking "problem" was felt to be only at two seasonal peaks, one at the height of the summer season, and the other during the Christmas shopping season. A count of spaces at potential satellite parking areas, such as the Town Hall, the library, the middle school, the elementary school, and Wilson Park showed 840 additional parking spaces available should satellite parking be needed.

The attractive Aesthetic appearance of the village is an important component in the development of Design Guidelines and one of the objectives of this Village plan. The visual "texture" of the Village is composed of such features as building architecture, paving and setbacks, street trees and fences, sidewalks, and the types of curbing utilized. The presence of a tree canopy, and other decorative plantings along the commercial and residential streets contributes greatly to the aesthetic character, as do signs, street furniture and the type of street lighting, employed. The variety of architecture styles contributes to the positive aesthetic quality of the village.



Updike Park in Wickford contributes to the aesthetic in the village.

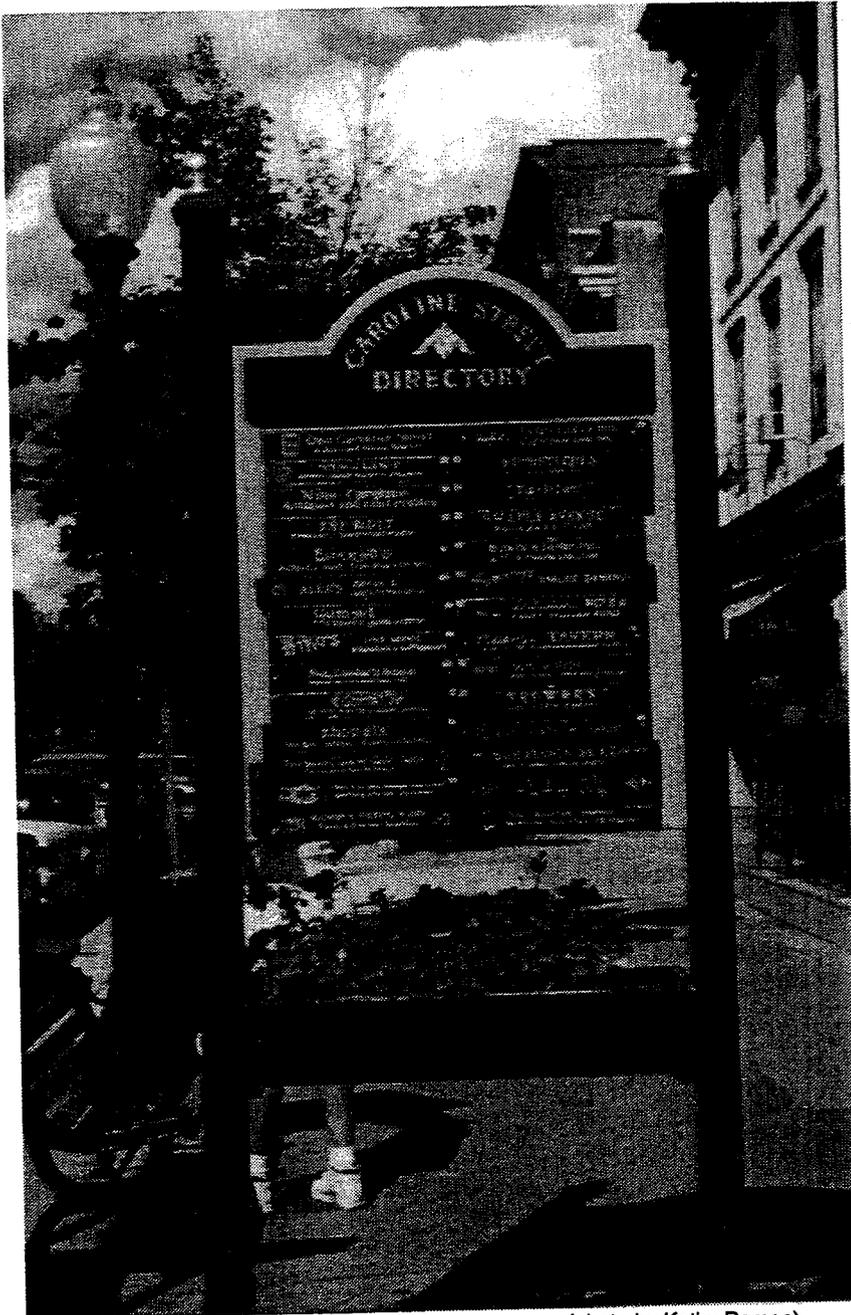
Quality of Life is probably the single most important trait of a place. It is the core question about places we visit, the bottom line when we consider a place where we would like to live. Quality of life goes hand in hand with good functionality. Functionality and aesthetics contribute to the quality of life. So too do good air and water quality, peacefulness; and the pleasing sights, sounds, and smells of a place. A livable place also provides the goods, services through local commerce that meet the community's physical and psychological needs. At the heart of the issue is "character"-that sum total of a place. Established commerce is important to the quality of life aspects of a community.

Survey Results. A survey of residents offered insights into the issues confronting Wickford Village. Respondents overwhelmingly supported protecting and enhancing the historic character of Wickford Village (94.9%), and were highly supportive of regulation to maintain that character (89.3%). A large majority (86.5%) affirmed that Wickford Village is a positive factor for choosing to live in North Kingstown. The average length of time that the respondents indicated they have lived in town is 19.1 years, which suggests that the respondents have a relatively long and happy sojourn in the village. New residents echoed existing resident's opinions about the Village.

Asked to identify three things they liked most about Wickford Village, the respondents most frequently cited (1) the variety of shops and services of a functioning village, (2) the friendliness and sense of community in the village, (3) the charm of the Villages historic character and architecture, (4) the harbor, waterfront and water views, (5) the sense of a small New England town, and (6) the pedestrian friendly quality of the village, where people shop, eat, browse, relax, work, socialize, attend festivals and concerts, and bring friends and relatives visiting from out of town.

Asked "what do you like and why do you visit," survey respondents offered suggestions as to improvements to the village to enhance the experience. Eleven categories of improvements were offered: landscaping, parking, crosswalks, traffic control, signage, building facades, lighting, waterfront walkways, bicycle access, shop diversity, sidewalks and other. The most frequently suggested improvements were (1) sidewalks, (2) traffic control, (3) more parking, and (4) the addition of waterfront walkways. Distant followers were landscaping, crosswalks, building facades, lighting, shop diversity, and signage. A few suggested that sewers were needed.

Other Issues. Signage was one of the concerns of the Wickford Merchants Association. Understandably, North Kingstown residents would be familiar with the village, but visitors from out-of-town would be more likely to need good signs, including direction to the public restrooms. A general directory is favored by the business community. Business signs already are regulated by a local Zoning Ordinance.



(photo by Kathy Romeo)

A directory sign in Saratoga Springs, New York provides information and adds to the visual quality of the area.

The Village Committee deems it important to maintain Wickford as a “working village”. In Wickford, the essence of a working village can be found in its diversity and the ability of the village to provide a range of services and residential options. Diversity

in the Village can be found in the architectural and open space along Main Street from the Town Wharf to West Main Street. There are sidewalks of Portland cement, exposed aggregate, brick, flagstone, and even unpaved. Widths of the sidewalks vary as do planting strips between the curb and sidewalk. Curbing materials are often discontinuous and street trees vary. There are mementos from the past, such as historic markers, WPA plaques and hitching posts, and along one section of street, the utility poles are actually located in the street. Some large street trees have root systems that have caused sidewalk pavement to tilt and buckle. Westerly from the intersection of Brown Street where Main Street becomes West Main, there are similar variations in sidewalk characteristics until sidewalks simply cease to exist on the southerly side of West Main. Along this section of roadway, graceful elms once provided a canopy over West Main Street, only to be destroyed by the great 1938 hurricane. The present trees are a mixture with many open spaces.



An inventory of sidewalk conditions showed deteriorated and unsafe sections in the village area.

Today, overhead utility poles and wires interfere with street trees. Regular pruning of trees to accommodate wires has disfigured the trees. A healthy full tree canopy is desirable, both from the perspective of visual aesthetics as well as improved environmental conditions resulting from shading, cooling, and air pollution mitigation. The relocation under ground of overhead utility lines in the village is a priority of the Village Plan Advisory Committee, to be accomplished through future public works projects, such as road reconstruction projects. This would be expensive, but one which

is often requested by communities in Rhode Island, but usually with little success. To overcome this cost impasse, a cost-sharing approach that would spread the financial burden among the RIDOT, the utilities and communication companies, and the locality would be worth trying. If they cannot be located underground, the lines should be relocated behind businesses or along residential back lot lines where feasible. On the pages that follow are some Wickford village scenes that demonstrate the effect of undergrounding utility lines.

At present Village street lighting is provided by overhead "cobra head" style fixtures and yellow light. This is functional lighting from the Rhode Island Department of Transportation's perspective. But, the Committee questions whether this is an attractive and the appropriate style for Wickford Village. Complicating the search for aesthetic and appropriate lighting is the fact that gaslights were never installed in Wickford Village.

Recognizing Wickford's long history as a seaport and harbor for recreational boating and the Narragansett Bay commercial fishing fleet, the Committee emphasizes the need to maintain and balance these uses. At the Town Dock, the commercial fleet consists of two small "draggers," a dozen lobster boats and 25 smaller shellfish boats. The Rhode Island Coastal Resources Management Council has approved the Harbor Management Plan, prepared by the Harbor Management Commission and the Department of Planning and Development, until the year 2001. The U.S. Army Corps of Engineers has also reviewed and commented on the Harbor Management Plan.

The Village Survey Questionnaire found that 25 percent of the respondents own boats and 20 percent do visit Wickford by Boat. These responses, the full rental of local marinas during the boating season, and the extensive waiting list for mooring space, cause the Advisory Committee to recommend that the various marina and shipyard facilities be maintained and protected by appropriate municipal ordinances.

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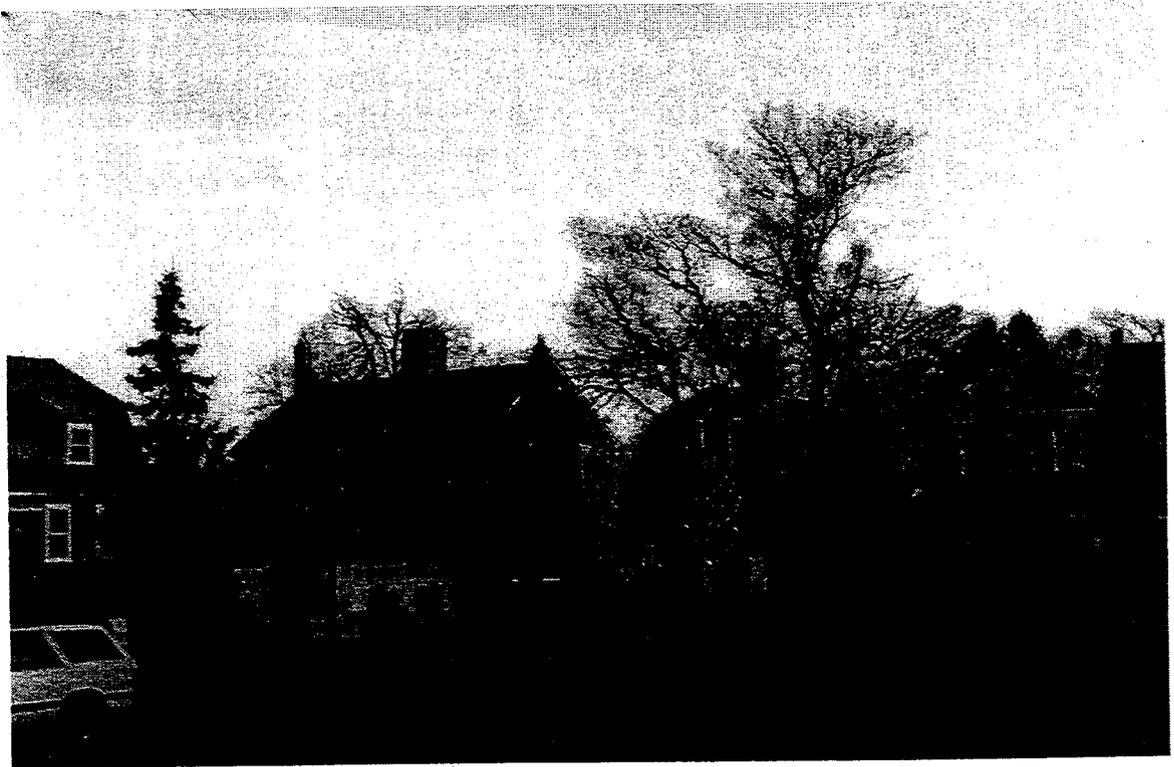
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Updike Park from Brown Street today.



Updike Park after utilities are buried (light fixtures are illustrative and not endorsed by the Committee).



Here a concrete light pole serves multiple purposes: overhead lighting, period lighting, and a support for hanging baskets.

Through these and other issues, Wickford merchants and residents work and live in an interdependent relationship that is recognized by both groups as special, if some times difficult. The merchants rely on a customer base that extends well beyond the village. On the other hand, there are issues associated with the special events held in the village and the impact on the residential neighborhoods. Residents have asked that public safety be assured and the impacts on quality of life be minimized. For instance, the Wickford Art Festival, one of the oldest and largest festivals of its kind, attracts tens of thousands of visitors to the annual event each July. The July Fourth fire

works, weekly concerts, the December Festival of Lights and other events that are planned or sponsored by the Town of North Kingstown, the Chamber of Commerce, the Wickford Merchants Association, and other civic groups are a concern of the village residents.

It is imperative that formal communications be maintained between groups to preserve harmonious relations, to manage village affairs, and to coordinate with the municipal government. This can be accomplished by creating a standing citizens advisory committee on Wickford Village activities to be composed of Merchants, residents representing a cross section of the village, and Town representatives to establish regular communication between the groups and to assist in the planning of special events.

National "Main Street" Program. One approach to planning in a working historic community is offered by the National Trust for Historic Preservation Main Street Program. Since 1980, the Trust has developed a program for the revitalization of more than 1,200 cities and towns in the United States. The "Main Street Four Point Approach," is not only a revitalization scheme, but is appropriate where a main street is already flourishing and can be applied in planning for the Village future. The four points can be summarized as follows:

1. Design. Improve the physical environment by renovating buildings, constructing compatible new ones, improving signs and merchandise displays, creating attractive and usable public spaces, and ensuring that planning and zoning regulations support the village goals and objectives.

2. Organization. Build communication and consensus among the broad range of public and private groups, organizations and constituencies.

3. Promotion. Market the village's assets to residents, visitors, investors and others through special events, retail promotion, and activities that improve the way the village is perceived, while maintaining the quality of life.

4. Economic Development. Strengthen the district's existing economic base while finding ways to expand its economy and introduce compatible new uses.

There are eight key principles to success: (1) comprehensive action; (2) incremental progress; (3) public-private partnerships; (4) action-oriented planning; (5) build on existing assets; (6) preserve and maintain quality; (7) change attitudes; and (8) self-help. There is ample evidence that these points and key principles are already being applied in Wickford Village. In fact Wickford is at a place in its evolution that most other places starting main street programs would seek to reach.

In Summary. Wickford Village cannot rest on its achievements. The Village is

developing as an economic center and attracting new residents. The purpose of this Village Plan is to show the way to build on the existing framework and achieve the overall goals. To foster a community that continues to be pleasing and functional.

Change in Wickford is occurring and will continue to occur. Some of this change will be the result of new population moving to North Kingstown and Wickford Village from other areas of the state. The prospective development of Quonset Point/Davisville where up to 22,000 new jobs may be created, will also bring change to the village and the Town.

Change will take other forms. There will continue to be public and private improvements; public infrastructure improvements, such as road reconstruction or utility work. This can be disruptive of daily life, it can even alter permanently the character of the village. Every effort must be made to see that it does not. Infill development, building rehabilitation or replacement, for instance, can similarly alter the character of the village. Changes made must be sympathetic with what is existing.

The loss of important natural or manmade features can be devastating to a community's physical appearance and very possibly on its social fabric as well. Something as simple as a tree tightly woven into a place and its thoughtless removal becomes a serious source of conflict. The anticipation, planning and management of these and other decisions are a prime concern of this Village Plan Committee. The improvement of communication between merchants, residents, and municipal government to meet these challenges is one of the priority recommendations of the Village Plan Advisory Committee.

## **Part 3.0 WICKFORD VILLAGE PLAN**

### **3.1 OVERVIEW OF THE PLAN**

The Plan that follows is composed of four primary components: the Goals, Objectives and Strategies that immediately follow; the Design Guidelines developed by ABACUS, INC. the Town's consulting firm for this plan; the Tree Survey; and the Intersection Improvements Concept Plans.

The **Goals, Objectives, and Strategies** have been crafted to form an Action Plan from which the Town Council and its agencies can identify the programs and initiatives required to implement the Plan, be it a public works effort, an ordinance or a business development incentive. The Committee envisioned that some implementation would be the responsibility of the business community in Wickford in partnership with the Town. The key elements of the Implementation program are displayed in a table format for ease of review in section 5.0.

The Plan presented here represents the consensus of ideas and concepts that have been developed by the Committee from, among others, research, business and resident surveys, the experience of other communities and the sense of what residents and business owners envision for Wickford. At the same time that this planning effort is completed, one of the elements of the goals and objectives and strategies is the notion that more planning, particularly in the form of design and engineering, is needed to fulfill the vision.

The Committee sought strategies that leveraged Town funds by maximizing outside funding opportunities. For example, the strategies include suggested improvements to Brown Street and its intersections with Phillip Street, West Main Street and Main Street; the anticipated public investment is considerable. This is a project, however, that could garner state transportation dollars particularly if the Town were to establish the design consensus and undertake some preliminary engineering work.

The **Design Guidelines** capture the community discussion about villages and how to protect the feel, the scale, and the sense of place. ABACUS, INC has developed a set of principles that include guidance for new development or redevelopment, for signage, for neighborhoods, for buildings and their layout, and for streets and their elements such as sidewalks, benches and other pedestrian amenities. Concept plans were developed for **Intersection Improvements** for four key Wickford crossroads. These improvements are intended to foster village scale, promote pedestrian activity, and offer traffic alternatives for through traffic by trucks and other large vehicles.

Town Tree Warden J. Peter Morgan who was also a member of the Committee conducted a Tree Survey. The results of that survey and the recommendations of the Tree Warden are incorporated within this Plan. A copy of the report is Attachment 2. The survey and report underpin the vision of a village greenbelt that is composed of parks, public spaces, and tree-lined streets.

### **3. 1 GOALS, OBJECTIVES, AND STRATEGIES**

**Goal #1.     *Enhance the historic character and vitality of Wickford village.***

#### **Objectives and Strategies**

##### **1. Village Center –**

- \*Recognize the importance of continuing to work with the Wickford Merchants Association and other community groups to enhance the Brown Street small business district.
- \*Use the design guidelines to facilitate building improvements;
- \*Maintain and improve safety through appropriate lighting;
- \*Emphasize the pedestrian orientation of the village;
- \*Encourage business to flourish through speedy permitting of improvements consistent with this plan;
- \*Provide incentives to encourage a storefront improvement program;
- \*Provide incentives for the maintenance and repair of historic buildings, such as making use of Historic Tax Credits allowed under R.I.G.L. 44.4.1;
- \*Consider expansion of the local historic district to include the village center;
- \*Use state Historical Preservation tax credits for building improvements in the historic district;
- \*Encourage careful maintenance of buildings;
- \*Utilize the "Main Street" program as a model for village improvements.
- \*Work to establish a postal satellite center in the village.

**2. Municipal Buildings and Public Works Improvements** – Develop and maintain municipal buildings and facilities, and state and local public works, consistent with the historic character of the village.

- \*Utilize building materials and designs that are appropriate to the village's historic character;
- \*Review all development projects with the local Historic District Commission when work is proposed within the Village for an advisory opinion;
- \*Follow the Village Plan goals, policies and design guidelines for improvements to public spaces;
- \*Coordinate the development of designs through the Department of Planning and Development;
- \*Develop a method of cost sharing to eliminate overhead utilities and installing electric, telephone, cable lines underground.

**3. Waterfront Facilities and Maritime Character** - Preserve and maintain the "working waterfront" of Wickford Village and enhance the public's enjoyment of the harbor:

- \*Maintain the Harbor Management Plan including the inventory of wharves, docks and bulkheads in the village area;
- \*Improve signage indicating points of interest along the waterfront;
- \*Create mini-parks with benches, but no parking, for pedestrian access along and at the end of public rights-of-way to the waterfront;
- \*Create a "boardwalk" along the waterfront in the center of Wickford Village;

- \*Support maintenance dredging operations as needed for the harbor and marinas;
- \*Test/explore Wickford as a destination for organized yacht club cruises;
- \*Expand the public transient\_docking facilities along the town dock behind the Town Hall Annex, adjacent to the public parking lot;
- \*Support the development of additional tourist destination facilities for boaters;
- \*Continue to work toward a “no-discharge” zone for boats in Wickford Harbor;
- \*Maintain and where feasible expand the availability of public launch facilities for small craft such as canoes and kayaks.
- \*Explore opportunities to repair the Wickford Harbor breakwater.
- \*Create an educational series for local boaters.

**Goal #2. *Improve the village’s Infrastructure, traffic and pedestrian circulation, parking, and alternative modes of transportation.***

**Objectives and Strategies**

- 1. Traffic - Regulate traffic flow through the village at speeds appropriate to a pedestrian-intensive commercial area and historic residential district:**
  - \*Utilize traffic calming techniques such as modifications to pavement widths, the use of varied pavement materials, pavement striping, planters, and intersection design modifications;
  - \*Investigate and where feasible, implement an alternative through route for Route 1A avoiding Brown Street;
  - \*Implement measures to reduce dependence on automobiles such as improving pedestrian and bicycle facilities and high occupancy vehicle use;

- \*Improve signage indicating points of interest along the waterfront;
- \*Create mini-parks with benches, but no parking, for pedestrian access along and at the end of public rights-of-way to the waterfront;
- \*Create a "boardwalk" along the waterfront in the center of Wickford Village;
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  - \*Investigate and where feasible, implement an alternative through route for Route 1A avoiding Brown Street;
  - \*Implement measures to reduce dependence on automobiles such as improving pedestrian and bicycle facilities and high occupancy vehicle use;

- \*Minimize through truck and bus traffic on Brown, Main, and West Main Streets;
- \*Continue public transit bus service to the Village;
- \*Where bus shelters are utilized, assure that the shelter design fits within the village architecture and appearance;
- \*Improve drainage to promote traffic and pedestrian safety;
- \*Investigate the feasibility of providing a bicycle route along the waterfront from the Clarence Hussey Bridge on Boston Neck Road to the Brown Street Bridge.
- \*Use Design Guidelines in developing intersection improvements.

**2. Parking** – Continue to provide convenient, attractive parking for residents and customers, adequate to the daily needs of the village; parking for special events should be planned so that vehicles are left outside the immediate village and visitors can reach events through alternative means:

- \*Reconfigure internal parking arrangements to create additional parking spaces;
- \*Make landscape improvements to parking lots to provide shading and to reduce storm water runoff and improve pedestrian accommodations in parking areas;
- \*Consider shuttles between satellite parking sites and the village center during special events;
- \*Improve bicycle paths/routes into and through the village and promote bicycle use, particularly during summer's peak traffic season;
- \*Inform visitors of additional parking and satellite parking locations during peak seasons and for special events;
- \*Encourage the use of satellite parking outside the village and at the periphery when appropriate;
- \*Investigate the use of the electric company's former substation site for additional parking.

**3. Pedestrians** - Encourage walking as the preferred travel mode within the Village. Every improvement should be planned and designed with the pedestrian in mind.

- \*Provide safe, attractive sidewalks where required;
- \*Where feasible, consider increased sidewalk widths in the village center to improve pedestrian movement and sidewalk seating;
- \*Consider narrowing pavement widths to facilitate pedestrian crossings, particularly at key intersections;
- \*Where feasible utilize changes in pavement materials for crosswalks;

- \* Consider the use of “Bump-outs” at crosswalks;
- \*Where intersections cannot be reduced in width, utilize pedestrian “refuge” areas by installing full or partial roundabouts or islands;
- \*Provide convenient unobtrusive trash/recycling receptacles at key locations along the sidewalks and where pedestrians congregate for sitting and/or viewing;
- \*Provide safe, attractive access to the waterfront and green spaces;
- \*Develop a waterfront walkway from the Brown Street Bridge to the Hussey Bridge;
- \*Investigate the development of a pedestrian way from Phillips Street along Academy Cove behind Wickford Elementary School to the Public Library;
- \*Provide street furniture for public seating;
- \*Provide bicycle rack facilities to keep bicycles from obstructing sidewalks;
- \*Utilize attractive materials and design variations along pavement edges and around street tree and utility pole cutouts;
- \*Explore the feasibility of the historic pathway between Fowler Street and Wilson Park, (with a connection to West Main Street), for low intensive local use.
- \*Investigate and document public access points to the waterfront.
- \*Investigate the possibility of developing a bicycle/pedestrian causeway from the Narragansett Electric substation to West Main Street.

**4. Green space** – Preserve the existing and provide new attractive green spaces throughout the Village, including street trees:

- \*Plant trees in locations providing sufficient area for tree roots such as root “vaults” to promote growth without destroying sidewalks and foundations;
- \*Utilize sustainable street trees that minimize conflicts with utilities and sidewalks;
- \*As a first priority for tree planting and tree maintenance, follow the recommendations of the tree warden and landscape architects that are certified arborists;
- \*Develop a tree replacement program;
- \*Institute a Town tree nursery to provide new and replacement stock in the village and elsewhere throughout the town;
- \*Identify and promote green spaces, parks, pedestrian walkways, and bicycle paths through appropriate signage and directory maps;
- \*Link green spaces with pedestrian walkways, paths, and bikeways
- \*Revitalize “Library Park” next to the Town Hall Annex with new landscaping, plantings, walkways, and views to the Harbor.

**5. Signage** – Distinctive village character signage to direct residents and

visitors to points of interest, shopping, parks, parking and the restrooms:

- \*Provide a centrally located "Village Map" with points of interest in a prominent place;
- \*Develop attractively designed and well-located directional signs to places of interest;
- \*Provide a community calendar bulletin board;
- \*Consider the use of historic informational and interpretive signs at public places;
- \*Eliminate sign clutter.
- \*Create distinctive appropriate signage.

**Goal #3. *Develop and maintain the balance in the residential and business community.***

**Objectives and Strategies:**

**1. Managing Village Activities** - Build upon the existing organizations to maintain communications between groups to enhance harmonious relations, to manage village affairs, and to work with municipal government.

\*Create a standing citizens' advisory committee on Wickford Village activities composed of merchants and residents representing a cross section of the village, to establish regular communication between the groups and to assist in the planning of special events

**Goal #4. *Anticipate, prepare for, and manage growth and change.***

**Objectives and Strategies:**

**1. Managing Village Growth** - Assure the desired balance between residential and commercial activities is maintained to preserve the quality of life enjoyed by village residents and to preserve, and to the extent practicable, enhance the economic vitality of the village as a place of business.

\*As an adjunct to the five-year update of the Town's Comprehensive Plan, maintain and update the Wickford Village Plan on a regular five-year interval;

\*Reconvene the Wickford Village Plan Advisory Committee to work with

the proposed standing advisory committee and the CompPlan advisory committee during plan update efforts;

\*Consider creating a design review committee for development plans that are not reviewed by the Historic District Commission

\*Maintain current densities of commercial and residential development;

\*Prevent the encroachment of commercial activities on residential areas;

#### **4.0 RECOMMENDED PRIORITY ACTIONS**

4.1. Brown Street Improvements. Among the many implementation strategies and actions identified above, the Wickford Village Plan Advisory Committee gave its highest priority for implementation to making improvements to Brown Street. Some of the improvements are structural in nature while others are designed to support the business community and protect the character of the village. While the focus of this priority recommendation is identified as Brown Street, clearly some of the recommended actions would naturally include the businesses on West Main, Phillips, and Main Streets. Specific improvements are listed and described below.

- Sidewalk, street intersection, street lighting and pedestrian amenity improvements are important to promoting the business community along Brown Street. Sidewalks on both sides of the street are in dire need of repair. Intersection improvements described in the Design Guidelines prepared by Abacus would narrow the intersections and allow for wider sidewalk areas and calming traffic at either end of Brown Street. Intersection improvements would require the approval of the State Department of Transportation. Street lighting appropriate for Wickford is recommended in place of the cobra lights now in place. Improvements to the sidewalk areas should include, as appropriate and feasible, the addition of trees and pedestrian amenities such as benches and the like. Where possible, utility lines should be placed underground or at the back of lots to improve the visual quality of the village. The Committee would recommend developing, through the Capital Improvement Program, an overall approach to accomplishing these improvements and a tentative schedule. As soon as possible, the project should be nominated to the State's Transportation Improvement Program.
- Protection of the community and historic character of the village depends on assuring that kind of protection for Brown Street, in addition to the local historic district. The Committee recommends the Town either:
  - 1) extend the local historic district to include Brown Street and those businesses at its intersection with Phillips Street; in the commercial center of

the village, Brown Street and Phillips Street are not included within the local historic district; or

- 2) Adopt a separate local district for Brown Street and appropriate areas of Phillips Street.
- Create a village greenway system through the redevelopment of Library Park, the development of a boardwalk along the water, and the addition of trees along Brown and other Streets in the village to link with Updike Park.
- Promote business programs and existing businesses using such techniques as
  - 1) the National Trust Main Street Program;
  - 2) Tax incentives for façade and sign improvements; and
  - 3) Working with the business community to evaluate and develop as possible additional parking sites.

4.2 Wickford Village Advisory Committee. The Plan recommends the creation of a standing advisory committee composed of residents, merchants, and Town officials to foster communication and cooperation among those with an interest in the village. Such a committee might meet several times a year or when special events or issues warranted.

4.3 Project Implementation Committee. This Plan provides an array of ideas and activities that the Town can undertake to improve, enhance, and protect the village of Wickford. The Plan recommends that the Town appoint a Project Implementation Committee who would be responsible for assisting the Town Council to implement the Plan. Such a committee could operate similar to the Updike Park Committee overseeing the development of projects and linking with the Town administration.

4.4 Develop a Project Portfolio. The Wickford Village Plan Committee recommends that the Town set aside funds in each capital improvement program for design and engineering consulting services for the projects described in this Plan. These funds are intended to advance at least several purposes:

- Develop estimates of project costs so that projects can be better scheduled in the Town Capital Improvement Program;
- Develop preliminary design and/or engineering for projects to facilitate the use of other funding sources such as those of the Rhode Island Department of Environmental Management or the Rhode Island Department of Transportation; and
- Develop project pictorial displays to promote the vision of the Plan and for

- the Town Capital Improvement Program;**
- **Develop preliminary design and/or engineering for projects to facilitate the use of other funding sources such as those of the Rhode Island Department of Environmental Management or the Rhode Island Department of Transportation; and**
  - **Develop project pictorial displays to promote the vision of the Plan and for public information.**

**PART 5.0 WICKFORD VILLAGE PLAN**

**5-YEAR ACTION IMPLEMENTATION PLAN SUMMARY**

Action	Description of Action	Implementation : Town Resource Agency	Estimated Funding	Estimated Year for Implementing	Potential Funding Source(s)	Purpose
Appoint Project Implementation Committee (PIC)	Create committee to oversee projects	Town Council	N/A	FY 98-99	Operating Budget	Facilitate implementing plan
Appoint Wickford Village Advisory Committee	Create a permanent committee to work with Town on Wickford issues	Town Council	N/A	FY 98-99	Operating Budget	Foster dialogue between Town, residents, and business community
Develop project portfolio	Set aside funds for design & engineering; schedule projects	PIC and Asset Management Commission	\$15,000+ per annum	Annually	CIP; grants	Develop good cost estimates; ability to pictorially display proposed vision; allows Town to prepare plans for submittal for grants, state agency construction, etc.
Nominate Brown Street/W.Main & Phillips St project To TIP	Town sends request to State Planning Program	Town Council, Planning Department, Planning Commission	N/A	FY98-99	N/A	Route 1A is a state highway; TIP placement would allow for state/federal funding of improvements
Sidewalk Program	Interim sidewalk repairs	Department of Public Works	\$25,000	FY98-99	CIP	Provide for needed repairs in advance of Main Street or Brown Street road construction programs

Action	Description of Action	Implementation : Town Resource Agency	Estimated Funding	Estimated Year for Implementing	Potential Funding Source(s)	Purpose
Wickford as a Yacht Club Destination	Develop programs to encourage Wickford as a destination for yacht club trips	Wickford Merchants, North Kingstown yacht clubs, Harbormaster	N/A	FY98-99	Business community	Encourages economic development without increasing vehicle trips and parking requirements
Bicycle Facilities	Provide bike racks at Annex/Library Park and Town Wharf	DPW	\$1500	FY98-99	RI DEM grant, operating	Encourage bicycle trips to Wickford and provide needed facilities
Village signage program	Develop a map and informational signage program	PIC, Wickford Merchants, Planning, Department of Public Works, HDC	\$4000	FY98-99	Wickford Merchants, Town	Improve informational and direction signage in village; may include business directory, restroom signage
Waterfront boardwalk and Library Park	Develop walkway along water and redevelop Library Park. Add to transit dock	Town Council, PIC, Planning, Wickford Merchants, DPW	\$200,000	FY98-99	RI DEM, Town, Wickford Merchants, PB Garden Club	Expand pedestrian facilities and amenities and access to the water. Expand transit boating facilities
Adopt local district for Brown St. and portions of Phillips St.	Develop ordinance	Planning Commission and/or Historic District Commission	\$5000	FY 99-00	Operating Budget	Protect the character of area outside local historic district

Action	Description of Action	Implementation : Town Resource Agency	Estimated Funding	Estimated Year for Implementing	Potential Funding Source(s)	Purpose
Brown Street Improvements:	Improvements including: sidewalks, lighting, add'l parking, trees, benches, underground utilities	Town Council, Asset Management Commission, PIC, Wickford Merchants	\$5000 benches trash recp \$50,000 design \$500,000 construct	FY99-00 FY99-00 FY00-01	Operating Budget CIP RI DOT, Tax Increment Financing (TIF)	Enhanced pedestrian amenities Prepare for construction program Construction
Postal Kiosk Center	Establish a location for a postal center either as kiosk or w/in a business	PIC, U.S. Postal Service, Wickford Merchants, Chamber of Commerce, Planning	N/A	FY00-01	N/A	Supports village as a place that provides range of resident and business needs.
Tree Nursery	Develop Town Tree Nursery	Town Council, Department of Public Works, Tree Warden	TBD	FY00-01	Town, grants, donations	Provide for a stock of street trees to be used in village and other parts of Town
Business District Façade Improvement	Establish program of incentives for façade Improvements	Wickford Merchants, HDC, Planning	TBD	FY00-01	TIF, grants, loan program, tax incentives	Promote improvements to facades in keeping with village character

Action	Description of Action	Implementation : Town Resource Agency	Estimated Funding	Estimated Year for Implementing	Potential Funding Source(s)	Purpose
Bike route link	Develop a bike route for crossing village	PIC, Planning, DPW	N/A	FY00-01	RI DOT	Provide safe link for bicyclists traveling into and across Wickford
Collation Corners Improvements	Reconstruct intersections to facilitate truck and bus movements from Phillips to Route 1.	Town Council, RI DOT, Department of Public Works, Planning	TBD	FY01-02	RI DOT	Encourage non-village traffic to use Tower Hill Road

## **ATTACHMENTS**

**ATTACHMENT #1  
WICKFORD VILLAGE DESIGN GUIDELINES  
BY ABACUS ARCHITECTS & PLANNERS**

**ATTACHMENT #2**

**THE TREE SURVEY OF WICKFORD**

**ATTACHMENT #3  
WICKFORD VILLAGE**

## **PARKING INVENTORY**

TOWN OF NORTH KINGSTOWN, RHODE ISLAND  
**Design Guidelines for Wickford Village**

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prepared by:

**ABACUS Architects & Planners**  
186 South Street, Boston, MA 02111  
245 Main Street, Wareham, MA 02571

APRIL 1998

## Design Guidelines Introduction

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GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines Introduction**

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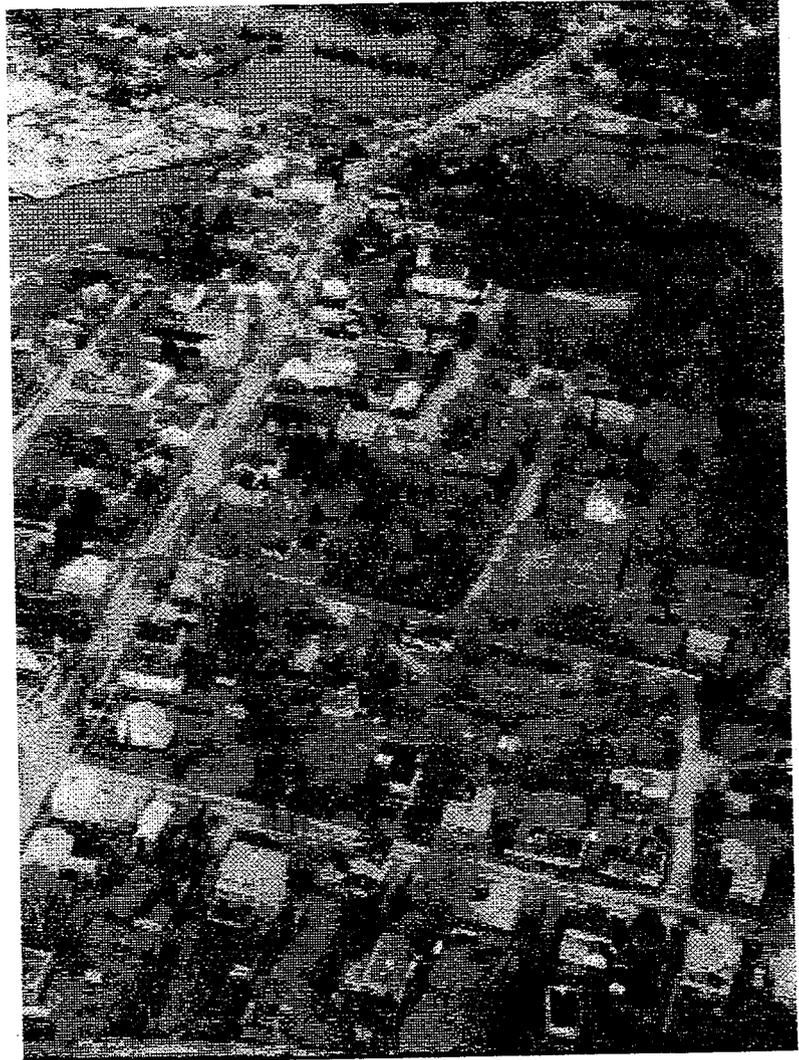
**PREFACE**

The Design Guidelines are divided into four sections which operate together: Design Guidelines for Neighborhoods, Design Guidelines for Streets, Design Guidelines for Building Lots and Design Guidelines for Buildings.

The overall layout and the interrelationship of different parts of the village are governed by the Design Guidelines for Neighborhoods. The design of streets is governed by the Design Guidelines for Streets. Small projects developing a single building lot are governed by Design Guidelines for Building Lots. All building projects are governed by Design Guidelines for Buildings. The guidelines serve as a reference for homeowners as well as business owners to utilize before commencing improvements to their individual properties.

These guidelines will insure that individual projects will be consistent with the overall goals of the community. They also enable the community to grow in both large and small increments, by the addition of whole neighborhoods or single buildings. They are designed to meet the flexibility needs of different scales of infill development. Larger projects would utilize all four sets of guidelines, while a simple building addition would refer to only one or two.

In Wickford Village part of the goal is to maintain the existing vitality of the commercial center without destroying the intimate scale.



*An aerial view of Wickford Village. (Photo courtesy of N. Kingstown Planning Department)*

## Design Guidelines Introduction

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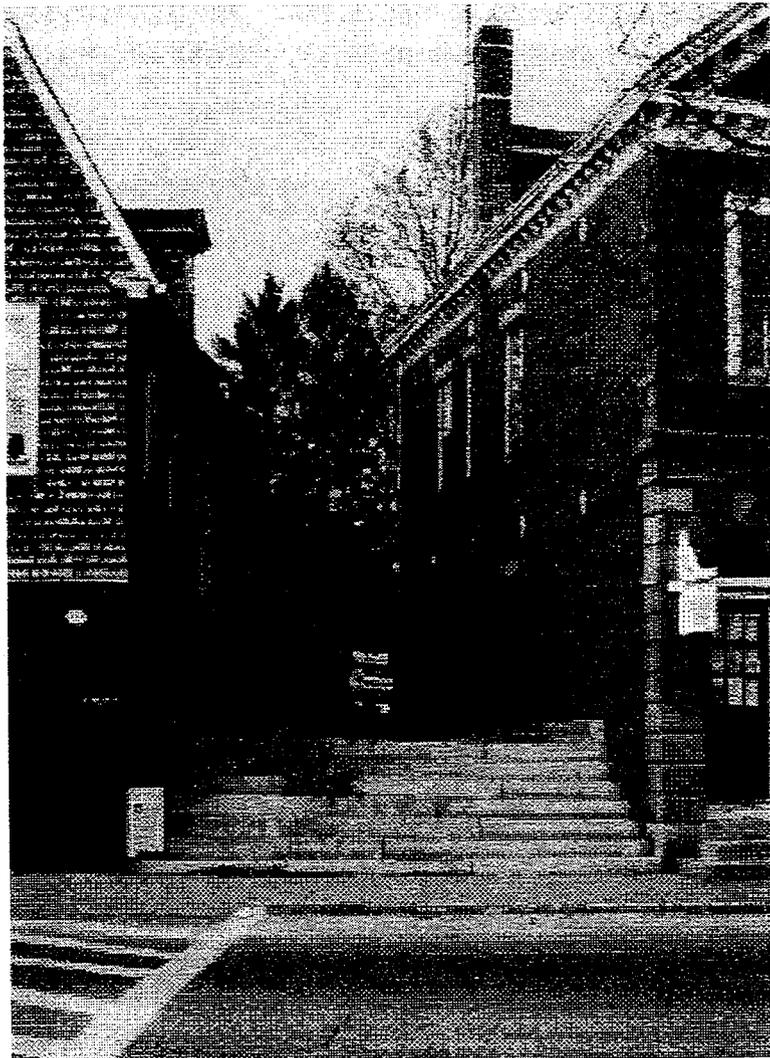
### PRESERVATION OF THE VILLAGE CENTER

#### *Guideline*

- \* In the center of Wickford Village every effort should be made to maintain the existing character through the implementation of these guidelines and the input of the community.
- \* The cooperation of the Planning Department, Town officials and private citizen groups such as the Wickford Village Association, Historic Wickford is strongly encouraged. Currently a portion of the Village is governed by Historic District zoning regulations. These guidelines supplement the existing regulations by covering the areas of the Village not governed by the Historic District.
- \* The guidelines should be utilized to the extent as feasible and practical by homeowners and business owners alike prior to commencing improvements.

#### *Discussion*

- \* Public participation in planning of projects is encouraged from early development.
- \* Communities are strengthened when all involved parties participate in open discussion.
- \* Regulations and guidelines are only one method for preserving the valuable assets of Wickford Village. The voluntary efforts of individuals and associations have always been the key to the maintenance of a successful village.



*Intimate spaces, such as this one created by the steps, serve as gathering spaces in the village.*

## Design Guidelines for Neighborhoods

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### GENERAL NOTES FOR NEIGHBORHOODS

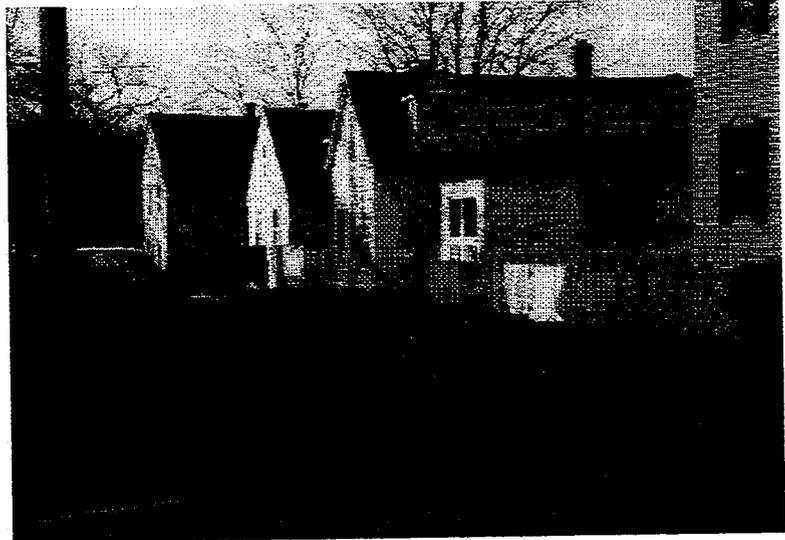
Wickford Village concentrates small scale commercial and residential activity into walkable mixed-use neighborhoods. The character of this village follows the patterns of the best aspects of traditional Rhode Island towns and consists of a close-knit pattern of varying sized lots, mixed-uses, interconnected streets and walkable, bikeable neighborhoods.

These guidelines promote the integration of appropriately scaled businesses and residences to increase the convenience and walkability of the village.

By encouraging incremental, centralized village scale development, Wickford can reduce the pressure to develop surrounding ecologically sensitive, rural and agricultural areas, without discouraging all new residential and commercial growth in the region. In addition, appropriate incremental development can accommodate needed types of housing such as for the elderly.

By contrast, suburban sprawl style development, with large lots and single uses, consumes open space, drives up the costs of housing and municipal services and requires that all local trips be done by car.

Carefully planned compact development with mixed-use buildings increases the vitality of the village.



*Appropriately scaled residences increase the walkability of the village.*

#### General Notes for Neighborhoods

Sites for Neighborhoods	1.2
Variety of Uses	1.3
Five Minute Walk	1.4
Connected Street Pattern	1.5
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## Design Guidelines for Neighborhoods

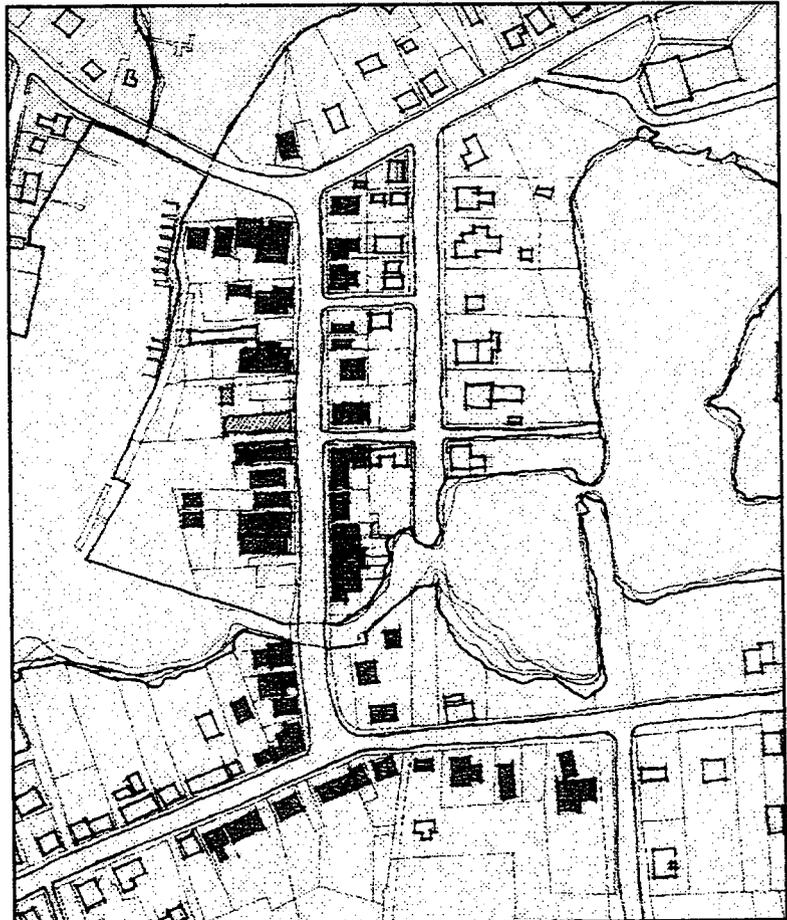
### SITES FOR NEW NEIGHBORHOODS

#### Guideline

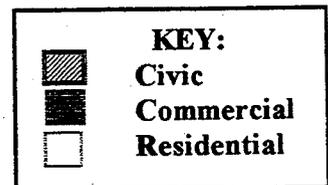
- \* A village center is intended to be the primary focus for retail commercial services. These include neighborhood retail, general business, light industry and offices which are conducive to rural community development.
- \* New neighborhoods should support and extend the distinct identity of the village, allowing surrounding areas to remain open and rural in character.
- \* They should not be located on prime agricultural land or sensitive ecosystems. The protection of waterways and aquifers and the preservation of scenic roads and rivers must be considered in locating and designing new neighborhoods.

#### Discussion

- \* These guidelines for new neighborhoods are based on traditional patterns of Rhode Island towns and villages, and are intended to perpetuate the best characteristics of those patterns.
- \* Village development can alleviate development pressures on working farmlands, wetlands and other rural open spaces by clustering new development in convenient, walkable mixed-use districts located near main road intersections.



The village center in Wickford Village includes neighborhood retail, general business, and offices which are conducive to community development.



## Design Guidelines for Neighborhoods

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### VARIETY OF USES

#### *Guideline*

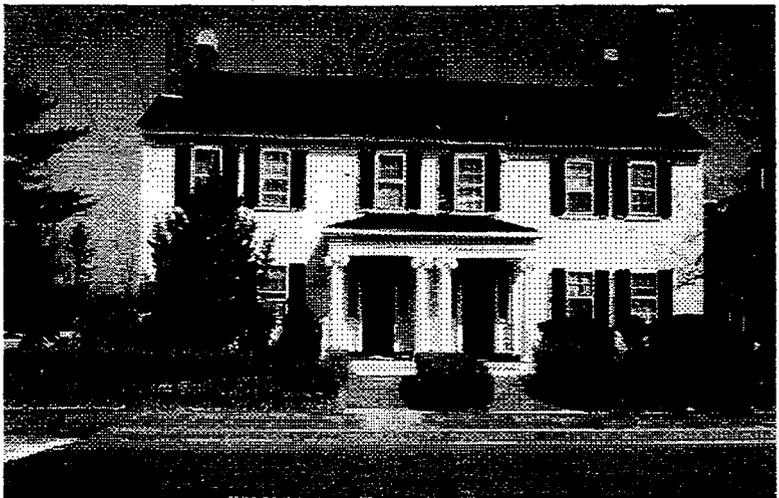
- \* Mixed uses are appropriate in the village center. Compatible uses may be located in adjacent or nearby buildings. Also, a single building may contain more than one type of activity.
- \* Residential dwellings or offices over commercial businesses can increase the vitality of the village center.
- \* Combined and adjacent uses should be compatible in terms of size, traffic generated and hours of use.

#### *Discussion*

- \* A closely mixed variety of uses is traditional in Rhode Island towns and provides residents with stores and services available near their homes. By contrast, a single use commercial district encourages large scale strip development with car-only access and large parking lots.
- \* Buildings designed to accommodate a variety of uses are more adaptable and can have a longer useful life.
- \* Residences over stores contribute to a lively village and increase the safety and the appeal of evening use. Parking lots can service more than one business if the schedules of use do not overlap, thus reducing overall parking loads.
- \* A mix of single family and multi-family houses increases affordability within a neighborhood and supports a variety of family structures.



*A commercial building with residential use on upper floors is encouraged in Wickford Village along Main Street.*



*A two-family house that is appropriately scaled can fit into an existing traditional Rhode Island neighborhood.*

## Design Guidelines for Neighborhoods

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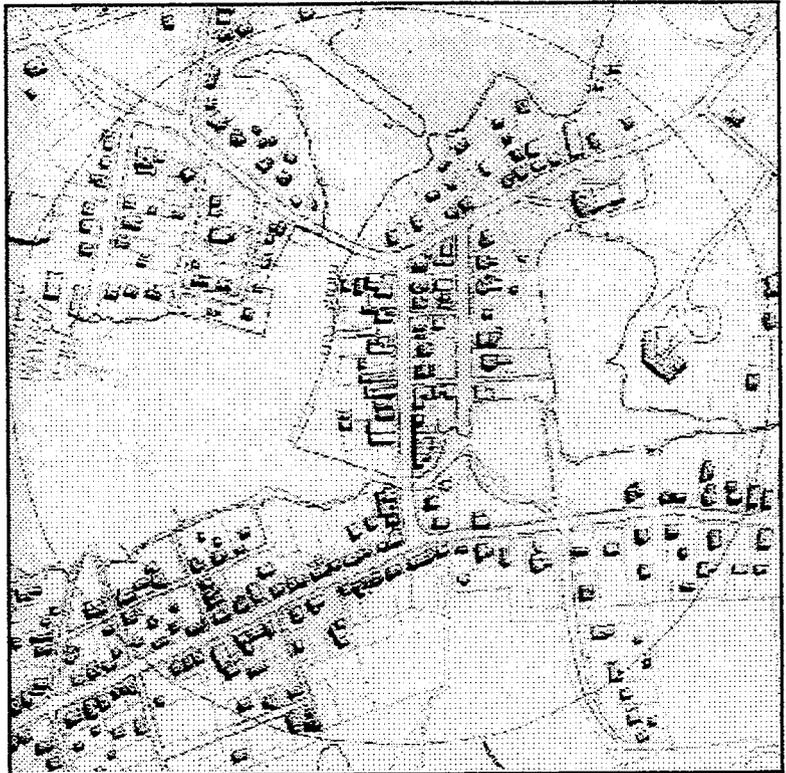
### FIVE MINUTE WALK

#### *Guideline*

- \* Villages like Wickford function as real communities because a high proportion of residences are within a five to ten minute walk (a quarter to a half mile) of commercial services and civic functions.
- \* Changes to the village should infill, enhance and extend the existing network of good walking paths. To continue to encourage pedestrian travel, walks must remain convenient, comfortable and interesting.
- \* Good sidewalks, views of destinations, attractive house facades animated by front porches, and interesting shopfronts all serve to entice walkers.
- \* Blank walls, parking lots, or undeveloped open lots interrupt a good walk and discourage pedestrians from continuing.

#### *Discussion*

- \* A successful village depends upon an active street life. Proximity and diversity are key to a walkable community. When services and activities are close many people- especially the young and the elderly who do not drive- have greater access and more ability to participate in the community.
- \* Research indicates that when a walk is longer than five or ten minutes most Americans will decide to drive instead, but if the distance is short and the walk is interesting most people will walk.



*Wickford Village center with five minute radius.*



*Intimate scale and interesting surroundings entice pedestrians down the GreeneWay. Walkability supports village life.*

## Design Guidelines for Neighborhoods

### CONNECTED STREET PATTERN

#### Guideline

\* New streets should connect in clear, direct and understandable patterns with existing streets meeting at right angles wherever possible. Dead-end streets and winding streets are appropriate only in response to topographical or wetland conditions.

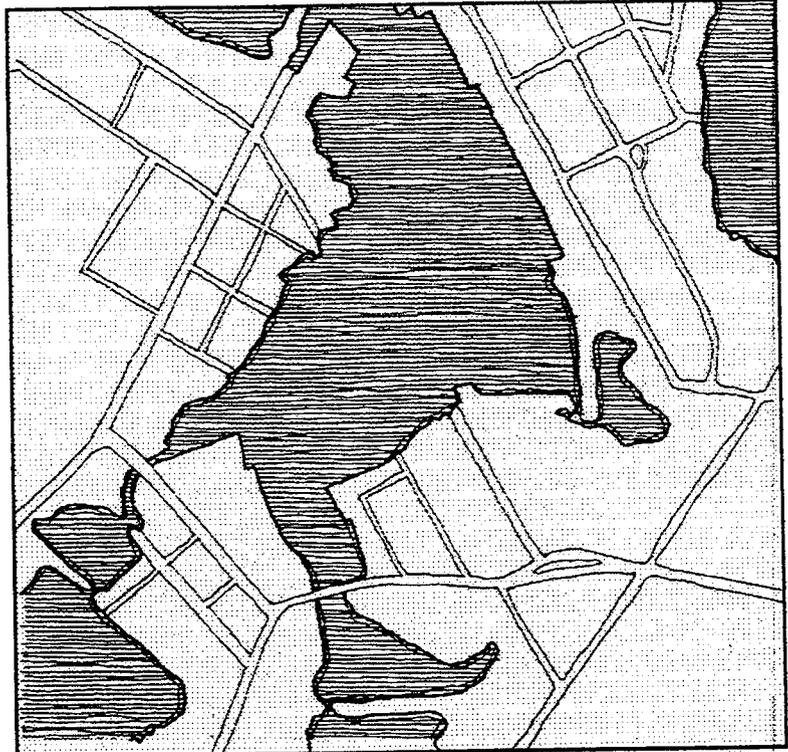
\* Where new development borders on open land expected to be developed, Rights of Way should be reserved for future street connections.

#### Discussion

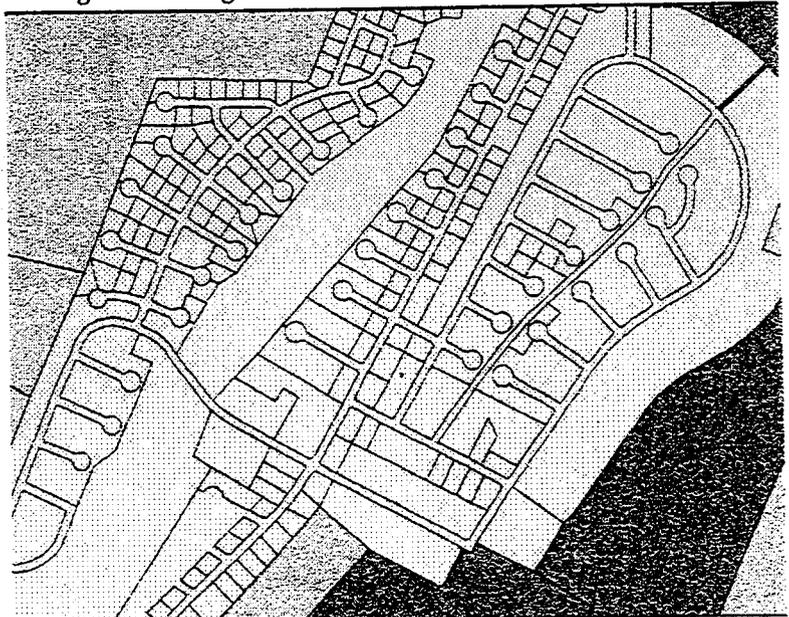
\* The connected pattern of streets is typical of historic Rhode Island towns. Simple, straight streets provide the shortest and most direct paths and make destinations visible. A direct and understandable street pattern encourages walking within a neighborhood.

\* Multiple routes through a neighborhood increase access for emergency vehicles without the need to create wider, faster streets.

\* Curvilinear cul-de-sac networks create indirect routes, increasing travel distances, gas consumption, and asphalt surfacing while discouraging walking and biking. All vehicular trips load the same principal roads, increasing traffic and resulting in wider main roads.



*Preferred: A direct and understandable street pattern encourages walking within a neighborhood.*



*Discouraged: Dead-end streets disconnect neighborhoods, overload primary roads, and consume more land.*

## Design Guidelines for Neighborhoods

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### VARIETY OF LOT SIZES

#### *Guideline*

\* Variations in lot sizes and frontage dimensions are encouraged. Lot frontages for detached houses in the village should be narrow, to encourage the walkability of the streets.

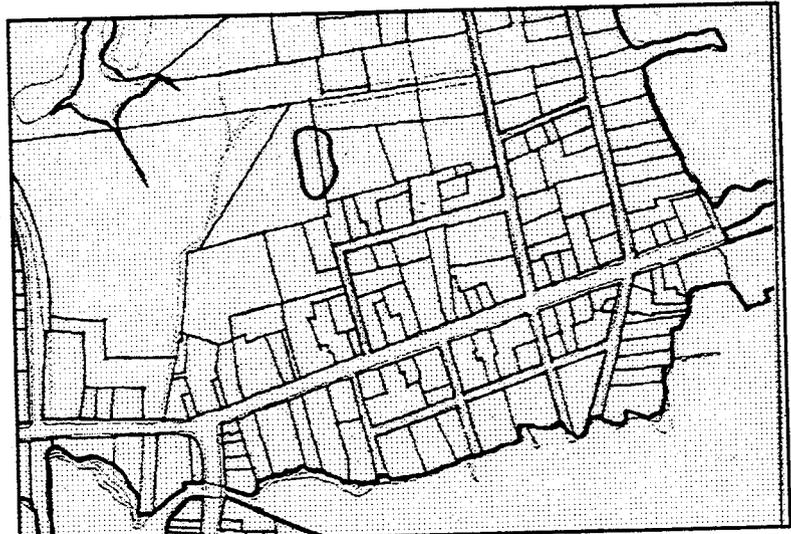
\* Rear lots on deep blocks can be accessed with 15 ft. street frontage. Rear alleys can service lots as narrow as 50 ft.

\* See *Guideline 3.2* for residential lot dimension setbacks.

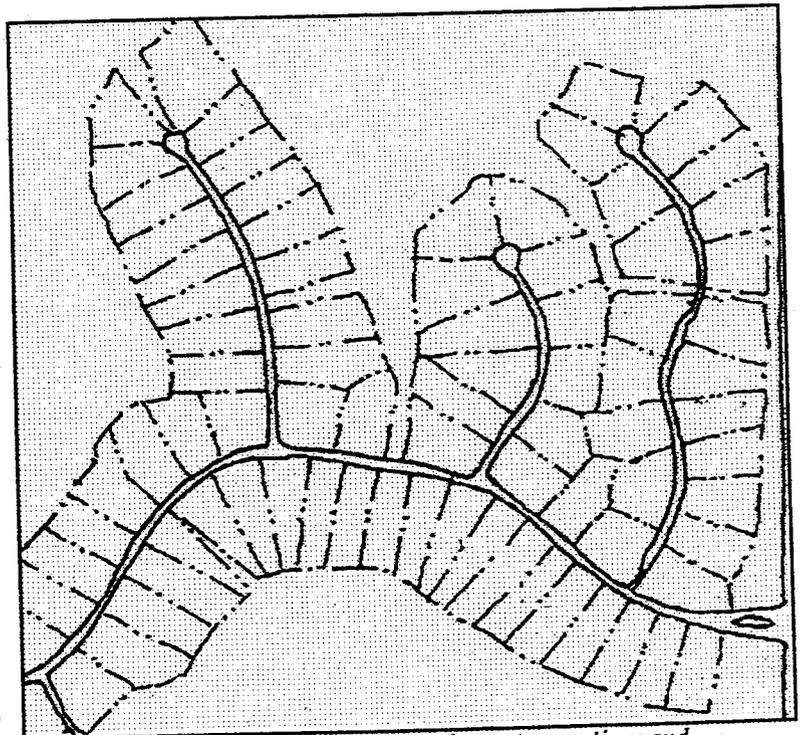
#### *Discussion*

\* The typical street in a Rhode Island town has a variety of lot sizes. This allows a range of house sizes and prices. Small frontage dimensions preserve the walkability and spatial character of the streets. The small lots make it possible for enough people to live in the center of town to make it a viable and vital area.

\* A variety of lot sizes encourages a variety of house types and sizes allowing for more mix of income ranges.



**Preferred:** A variety of lot sizes allows a range of house sizes and prices.



**Discouraged:** Uniform lot sizes limits house type options and encourages suburban sprawl.

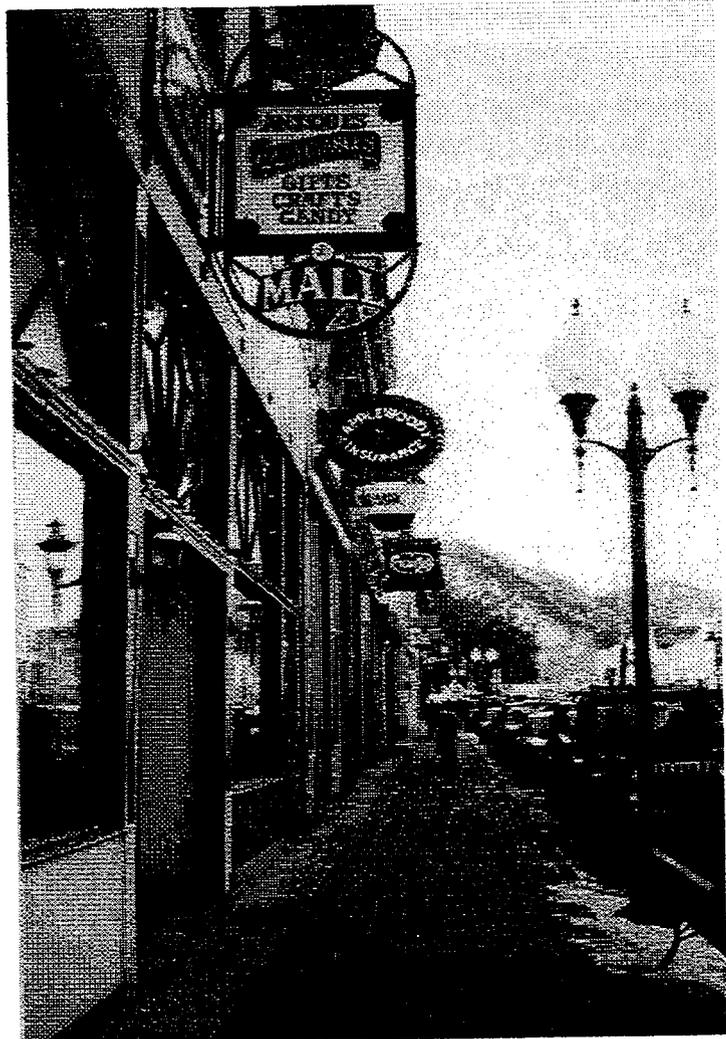
## Design Guidelines for Neighborhoods

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### SIGNAGE

#### *Guideline*

- \* Signs in neighborhoods should blend with the existing character of the individual area, whether it is residential or commercial.
- \* Directional, informational and safety signs should be designed, grouped and organized so as to be recognizable, legible appropriate and uncluttered in appearance.
- \* Business signs should be designed to enhance the architectural character of the Village. Business signs should be attached to buildings, porches, or fencing.
- \* Awning signs are encouraged.
- \* Signs projecting perpendicular to a building's face should be no more than 6 ft. tall and project no more than 6 ft. beyond building wall. Fabric banners may be taller, but not more than 3 ft. wide.
- \* On a lot with a building at the front lot line, freestanding signs are not allowed.



*When appropriately designed, signage can enliven a shopping street and enhance its architectural character.*

#### *Discussion*

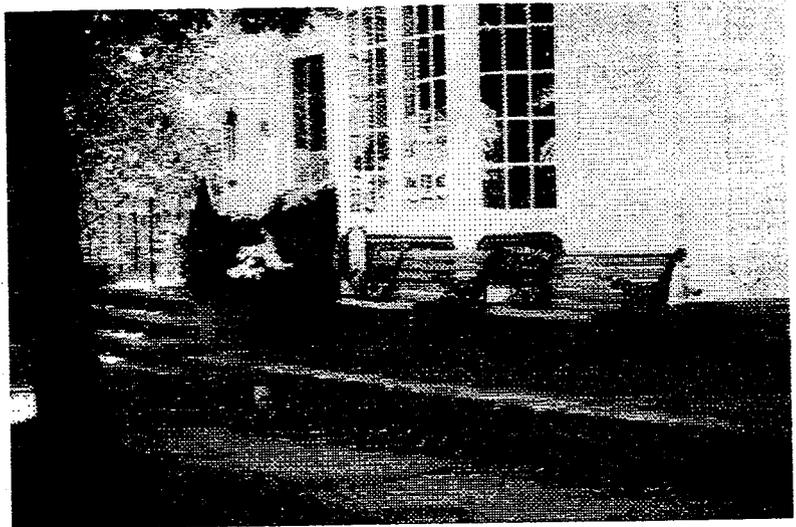
- \* A variety of business and commercial signs can contribute to a vital and interesting streetscape.

## GENERAL NOTES FOR STREETS

Streets are the fundamental public space of a community and should be pleasant spaces to occupy.

These guidelines are intended to create streets which function as attractive and useful public spaces, and which serve the needs of pedestrians, cyclists and automobile drivers.

Based on the best features of traditional Rhode Island towns, the guidelines insure that new development extends the character of the village.



*Wickford Village has charming shopping streets.*

### General Notes for Streets

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## Design Guidelines for Streets

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### STREETS FOR SHARED USE

#### *Guideline*

\* Streets should be designed to provide for pedestrians as well as vehicles, bicycles, wheelchairs and personal carts and carriages.

\* Adequate and, where necessary, separated travel-ways for bicycles and pedestrians should be included in all designs for new streets and major renovations.

\* Where design conflicts arise, precedence should be given to the safety and comfort of the pedestrians.

#### *Discussion*

\* Traffic calming techniques can make streets more hospitable for pedestrians. Narrow streets and right-angle turns slow traffic. Parallel parking along streets calms traffic and makes streets safer. Raised textured crosswalks create designated pedestrian space.

\* Multiple travel paths disperse traffic and allow for narrower and safer streets.



*Preferred: Streets and sidewalks to provide for pedestrians and carriages.*

## Design Guidelines for Streets

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### STREET WIDTHS

#### *Guideline*

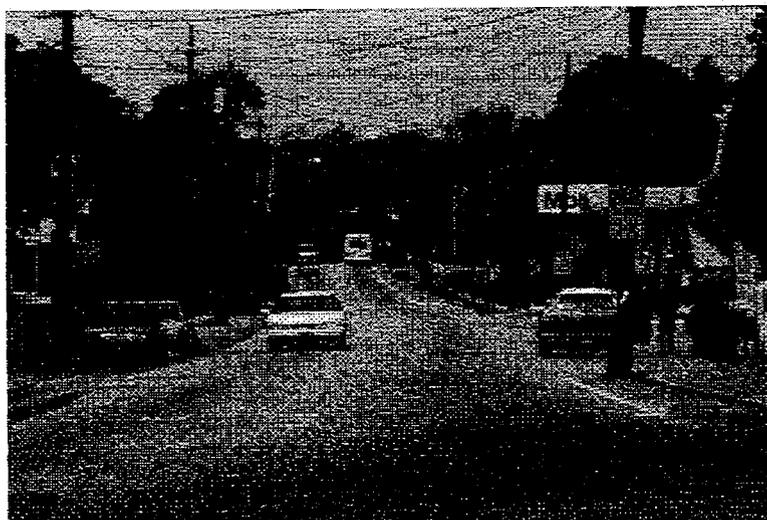
- \* The paved portion of streets should be narrow. New streets within the village should be designed for relatively slow speeds.
- \* Street widths and profiles should be different in the village, on residential streets in flat areas and on sloped terrain.
- \* In general, in commercial areas, street width should not exceed the narrowest width of Brown Street.

#### *Discussion*

- \* Streets designed for slower moving traffic are safer and encourage walking and the use of the street as shared space.
- \* Wider streets encourage higher speed vehicle travel endangering pedestrians and cyclists.



*A narrow residential street is preferred as it serves to slow traffic and encourage walking.*



*Wider streets encourage higher speed vehicular travel endangering pedestrians and cyclists. (Photo courtesy of N. Kingstown Planning Department.)*

## Design Guidelines for Streets

### STREET SECTION FOR VILLAGE CENTER

#### Guideline

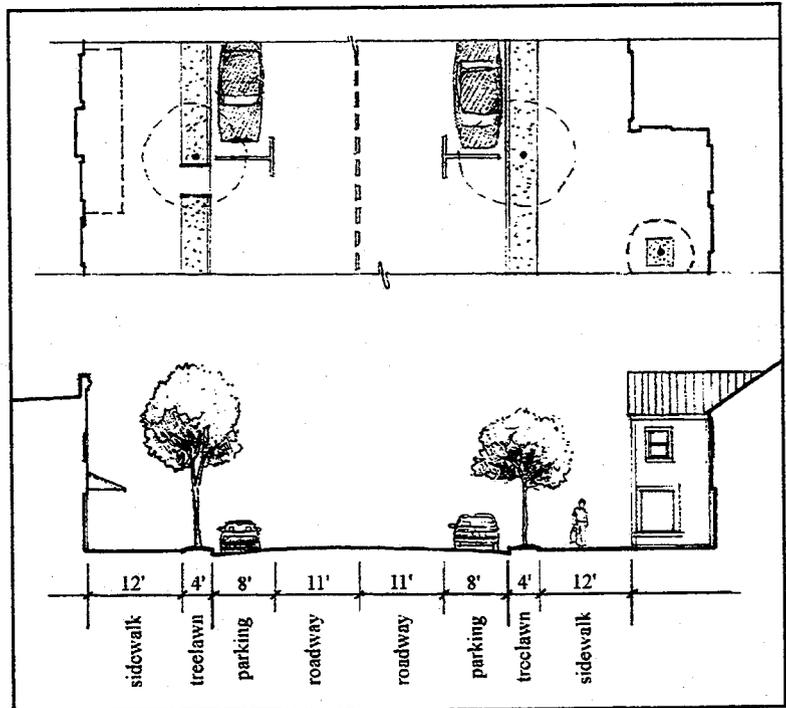
\* The paved portion of streets within the village center should be no wider than necessary while accommodating cars, service, emergency vehicles, and on-street parking. New streets should be designed for relatively slow speeds.

\* Major shopping streets within the village center should provide two 11 ft. driving lanes and 8 ft. parking lanes on both sides. Side streets can include perpendicular parking with two driving lanes.

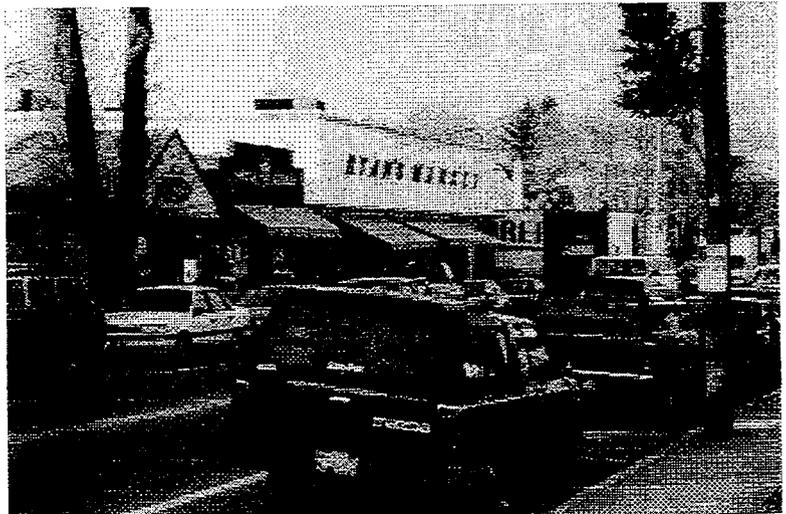
\* Rights-of Way should include space for on-street parallel parking on both sides, sidewalks, bike lanes where appropriate, and tree planting.

#### Discussion

\* Streets designed for slower moving traffic encourage walking and the use of the street as shared space.



*Slow traffic streets make good pedestrian places.*



*A shopping street in Wickford Village with parallel street parking on both sides of the street.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Streets**

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## SIDEWALKS IN VILLAGE CENTER

### *Guideline*

- \* A continuous system of sidewalks should be clearly delineated across driveways, ramped at intersections and wide enough to accommodate the expected pedestrian traffic.
- \* Sidewalks should be 12 ft. wide along both sides of the street. Sidewalk design should accommodate wheelchairs, small personal shopping carts, and baby carriages. Sidewalk design should incorporate amenities such as street trees, planters, seating and awnings. Where 12 ft. wide sidewalks are not feasible 8 ft. wide sidewalks are acceptable.
- \* Hard curbs are desirable in the village commercial areas. All intersections should have ramped access.
- \* Sidewalks should be made of concrete, brick, or stone (bluestone or granite). A variety of patterns is encouraged. Asphalt sidewalks are not allowed.
- \* The material of a sidewalk should continue across curb cuts and alleys.

### *Discussion*

- \* Continuous sidewalks encourage walking and window shopping. Wide sidewalks create more comfortable pedestrian spaces and allow for amenities like street trees, planters, seating and awnings. Sidewalks that accommodate carts, carriages and wheelchairs encourage a variety of citizens to travel without cars for short distances.



*Good sidewalks create comfortable pedestrian places and encourage window shopping. (Photo courtesy of North Kingstown Planning Department)*

## Design Guidelines for Streets

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### PARKING STANDARDS IN VILLAGE CENTER

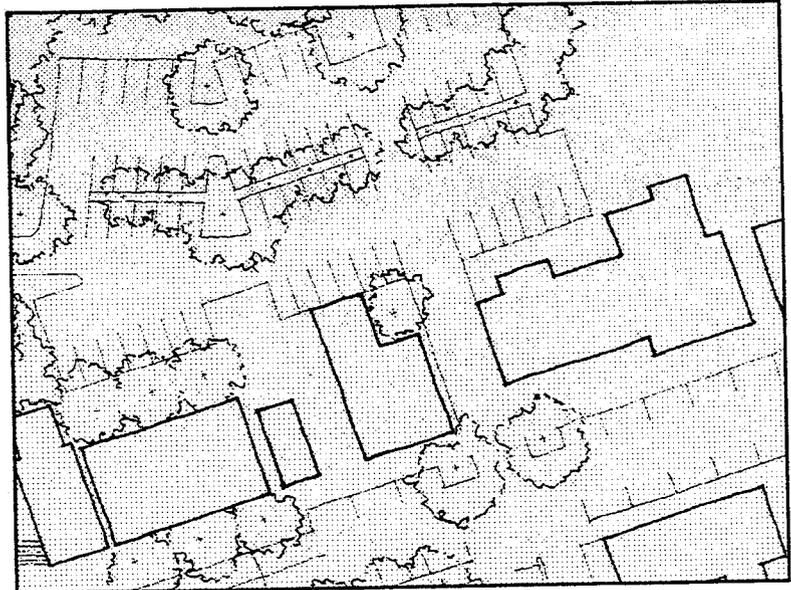
#### *Guideline*

- \* Within Wickford Village, off street parking requirements must be adjusted to reflect the successful pedestrian quality of the village.
- \* New and adaptive uses must submit a parking proposal, including a site plan with designated spaces, (and landscape design) adjacent on-street spaces and negotiated arrangements for shared parking with nearby properties. In addition, a proposal for handling higher than expected parking loads may be necessary. This will be evaluated with owner's projected parking loads to determine whether to override N. Kingstown standard requirements.

\* See also *Guideline 3.4.*

#### *Discussion*

- \* Adjusting off-street parking requirements to reflect pedestrian quality allows for the character of the village to remain intimate, while accommodating large parking loads.



*Connected and landscaped parking lots can accommodate several properties, thereby increasing the vitality of the village.*

## Design Guidelines for Streets

---

### ON-STREET PARKING IN VILLAGE CENTER

#### *Guideline*

- \* Parallel parking should be permitted on both sides of major streets, with a parking lane provided on each side.
- \* Adjacent on-street parking counts toward meeting non-residential parking requirements.
- \* The parking lane should stop 20 ft. before an intersection and the sidewalk should widen to reduce the street crossing distance (see *Guideline 2.14* and *2.15*)
- \* On-street parking should never interrupt the continuity of the sidewalk.
- \* Where diagonal parking is permitted, back in spaces are preferred.



*Parked cars create a protected pedestrian space.*

#### *Discussion*

- \* Parked cars on the street help to slow traffic and reduce the need for parking lots. They also offer convenient parking and encourage pedestrian use of the streets and other retail areas by dispersing parking and creating a safety buffer between the sidewalk and the street.

## Design Guidelines for Streets

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### ALLEYS AND PARKING LOTS IN VILLAGE CENTER

#### *Guideline*

- \* Parking lots should be linked by alleys or direct off street connections. One tree for every eight parking spaces is required.
- \* Alleys may function as back streets in the village center.

#### *Discussion*

- \* The addition of alleys greatly improves access to parking lots located behind street retail and mixed-use buildings.
- \* Alleys ease congestion and allow more on-street parking by reducing driveway curb cuts. Parking serviced by alleys also allows smaller street frontages and a more continuous street edge.
- \* Connected alleys facilitate parking behind buildings.



*Alleys ease congestion, allow more on-street parking, and function as back streets in the village center.*

## RESIDENTIAL SIDEWALKS

### *Guideline*

\* A continuous system of adequate sidewalks should be clearly delineated across driveways, ramped at intersections and wide enough to accommodate the expected pedestrian traffic.

\* Sidewalks should be at least 4 ft. wide. Sidewalk design should accommodate wheelchairs, small personal shopping carts and baby carriages.

\* On low traffic, one-way streets or streets with only one lane of parallel parking, sidewalks should be on the parking side. On major residential streets, sidewalks should be on both sides.

\* A variety of sidewalk materials and patterns using brick, concrete, and stone are encouraged.

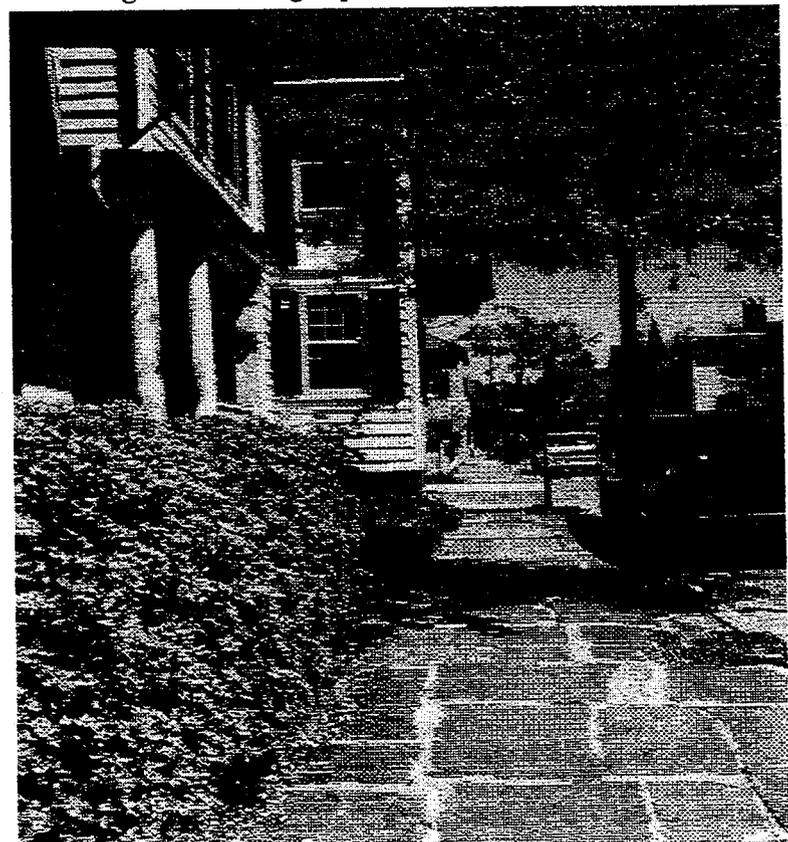
### *Discussion*

\* Continuous sidewalks encourage walking in residential areas. Sidewalks that accommodate shopping carts, carriages and wheelchairs encourage a variety of citizens to travel without cars for short distances.

\* Continuous sidewalks that link residential areas to the commercial center promote a successful village. The ability to travel from their home to shopping and business areas encourages citizens to be involved in their neighborhood.



*Sidewalk proportions and relationships among the parts; sidewalk, street trees, parked cars, are illustrated here. (Photo courtesy of the North Kingstown Planning Department.)*



*A variety of paving materials enhance the character of residential neighborhoods.*

## Design Guidelines for Streets

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### RESIDENTIAL ON-STREET PARKING

#### *Guideline*

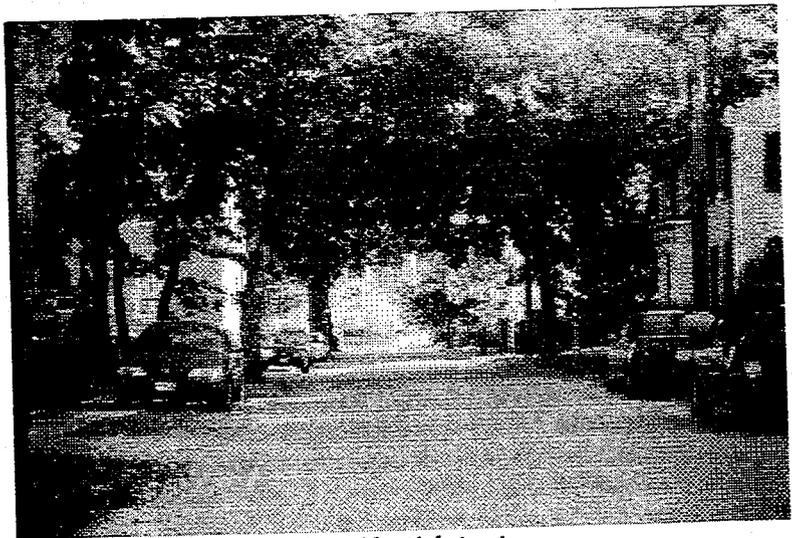
\* Parallel parking on residential streets should be provided in an expanded gravel shoulder on at least one side of the street. Adjacent on-street parking provides guest parking for residences and may reduce off-street requirements to not less than 1 space per dwelling unit.

#### *Discussion*

\* Parked cars on the street help to slow traffic. They offer convenient guest parking and encourage pedestrian use of the streets by creating a safety buffer.



*Parked cars on the street help to slow traffic.*



*Parking on soft shoulder of a residential street.*

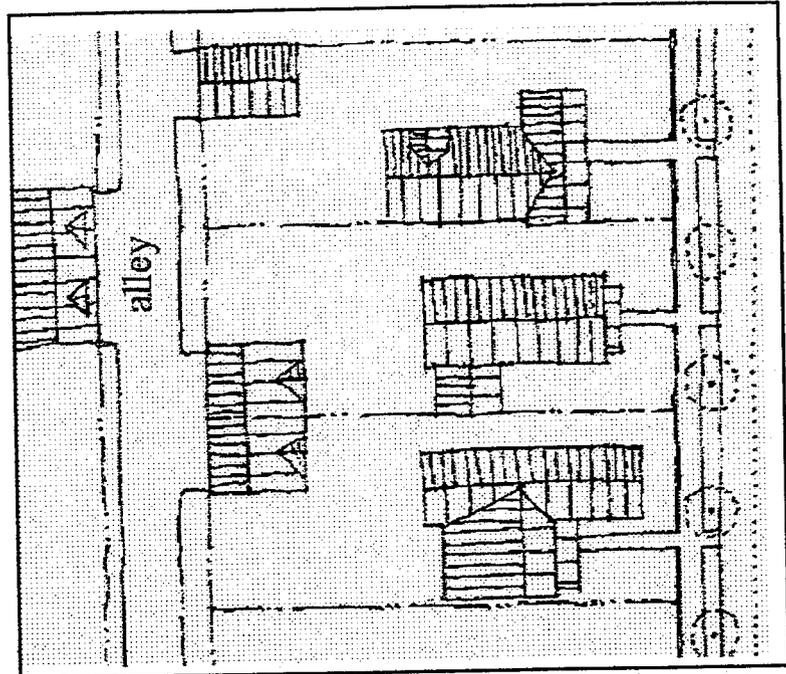
## RESIDENTIAL ALLEYS AND PARKING LOTS

### *Guideline*

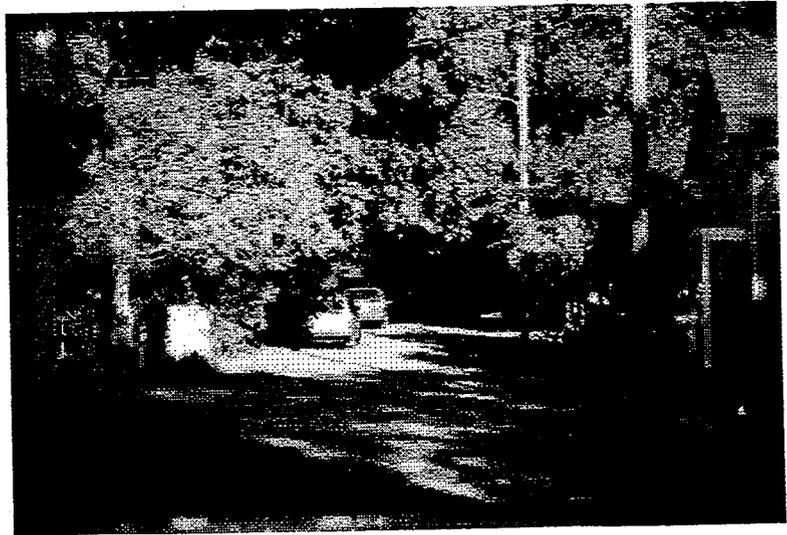
- \* Where new lot frontages are 50 ft. or less, provide rear alleys for access to garages behind houses.
- \* Alleys should have a maximum of 12 ft. of paving, with a Right-of-Way of 20 ft. Wherever possible they should be unpaved or paved with gravel.

### *Discussion*

- \* Rear alleys allow direct access to garages behind houses, maintaining a continuous sidewalk and streetscape for pedestrians.
- \* Trash and recycling can be handled from alleys further improving the quality of the street.



*Where lot frontages are 50 ft. or less, provide rear alleys for access to garages.*



*Residences with rear alley access to garages.*

## Design Guidelines for Streets

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### STREET TREES

#### *Guideline*

- \* Streets in the village should be lined with trees. Existing trees should be preserved where possible.
- \* On commercial streets indigenous species with broad canopies should be planted to create a continuous canopy. Monocultures should be avoided in large areas.
- \* Street trees should be planted at regular intervals of no more than 30 ft.
- \* Existing mature trees should be preserved where possible to serve as street trees or to shade public plazas or parks.
- \* For recommended species and standards see report by N. Kingstown Tree Warden.

#### *Discussion*

- \* Street trees give a special character to neighborhood streets. The use of a specific kind of tree for each street is one strategy for enhancing this character.
- \* Street trees provide summer shade and reduce heat build-up from asphalt areas. They create pleasant walking spaces for pedestrians.



*Preferred: Shade trees encourage walking.*



*Shade trees in conjunction with a variety of paving materials maintain the special character of neighborhood streets. (Photo courtesy of the N. Kingstown Planning Dept.)*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Streets**

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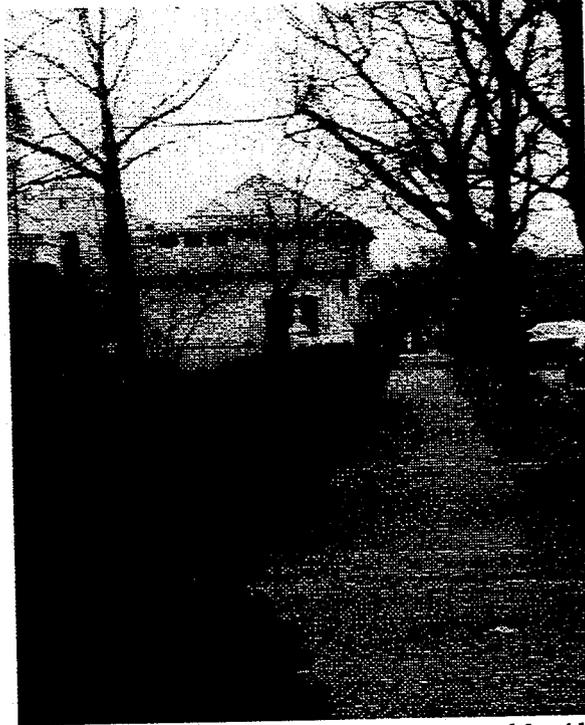
## TREE LAWNS

### *Guideline*

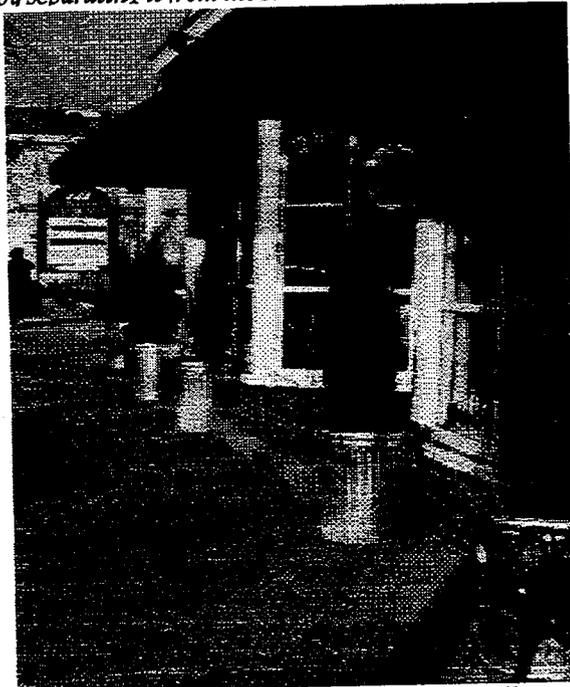
\* Tree lawns (planting strips in the Right-of Way between street and sidewalk) are desirable to separate sidewalks from parking and driving lanes. Typically, they should be 4 to 6 ft. wide, planted with grass or other ground cover and trees.

### *Discussion*

- \* Tree lawns create a transition between the street and the sidewalk and make curbs unnecessary.
- \* On commercial streets tree lawns and boxes can contribute to the intimate village scale.



*Trees lawns help define the pedestrian space of the sidewalk by separating it from the street.*



*These potted trees contribute to the intimate village scale of the commercial area in Wickford Village.*

## INTERSECTION DESIGN

### *Guideline*

- \* Intersections should be designed to facilitate the flow of traffic in the most desirable direction and to slow the flow onto residential streets.
- \* In town, priority should be given to the pedestrians.
- \* Pedestrian crosswalks should be designated in a material contrasting with that of the street, preferably brick or cobblestone. This delineates the pedestrian's line of travel. It also alerts motorists of the upcoming change and causes them to slow down.

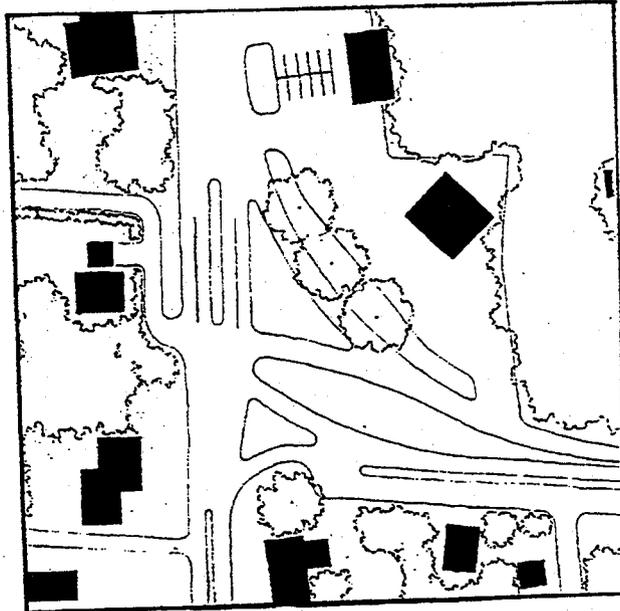


*Pedestrian crosswalks should be designated in contrasting material to alert motorists of the upcoming change in pattern.*

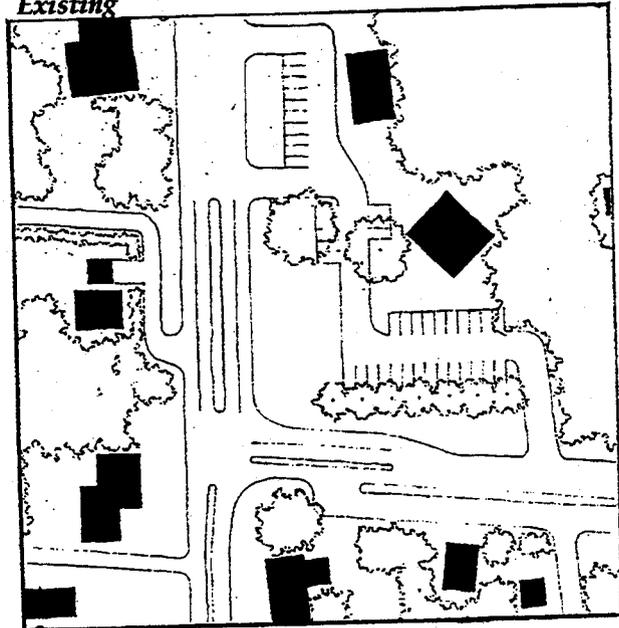
## POST ROAD, TOWER HILL ROAD, AND WEST MAIN STREET

*Discussion*

\* The proposed layout attempts to allow Wickford Village to have an attractive small scale character for roads into the village. While maintaining the turn lanes, the overscaled sweep of them is reduced; this creates a new site for a building close to the corner and more clearly defines the intersection as a gateway to the Village.



*Existing*

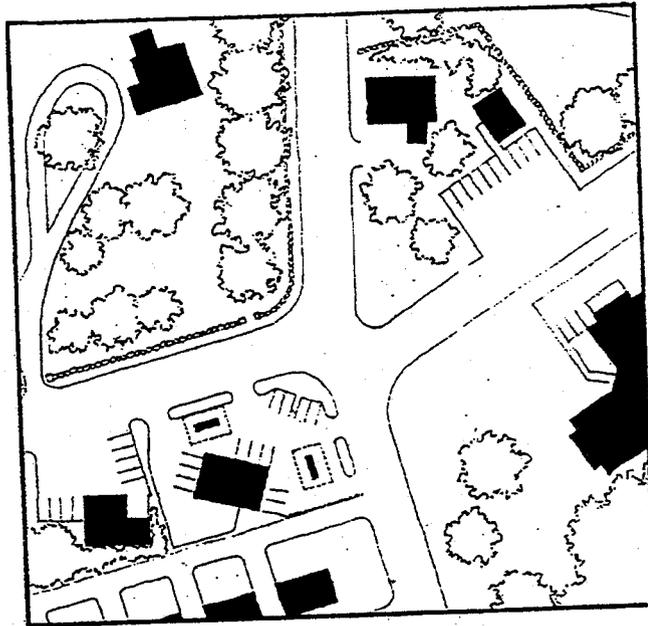


*Proposed*

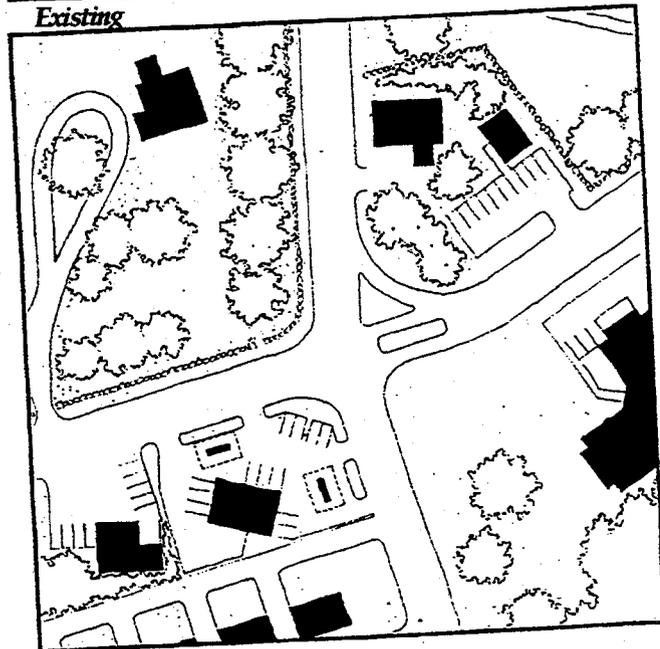
## TEN ROD ROAD, TOWER HILL ROAD, AND PHILLIPS STREET

### *Discussion*

\* This intersection is another primary gateway into Wickford Village. Currently it impedes traffic flow with its misalignment of roads and oblique approach to Route 1 from the East. The proposed intersection creates a safer 4-way crossing with a turning lane and landscaped islands. Access to parking is controlled by clear curb cuts with landscape buffers.



*Existing*



*Proposed*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Streets**

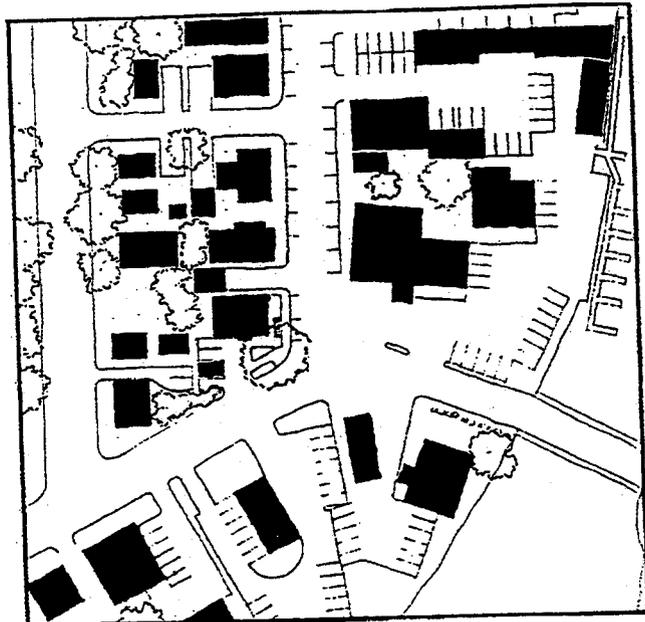
**BOSTON NECK ROAD, PHILLIPS STREET AND BROWN STREET**

*Discussion*

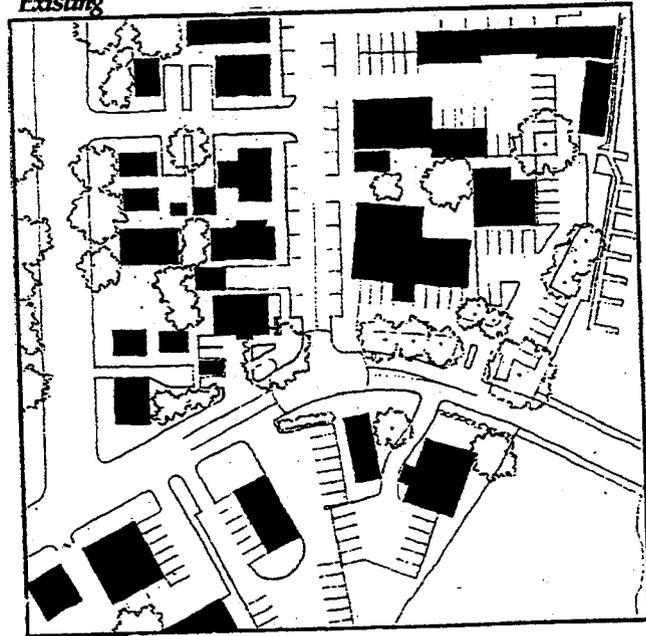
\* This intersection marks the entry into the pedestrian shopping area of the village. Presently it is confusing and dangerous with an ill-defined hierarchy of rights-of-way. It is inadequate as an introduction to a great shopping street.

\* The new layout encourages the connection of through traffic between routes 1 and 1A on route 102 rather than through the village. The redesign provides important definition of a pedestrian corner to the NE reinstating the sidewalk and adding trees. The streets are narrowed at points of pedestrian crossing for greater safety.

\* The boundaries of the intersection are defined with paving material intended to calm traffic. Pedestrian access to the waterfront is established and existing parking lots are connected and landscaped.



*Existing*

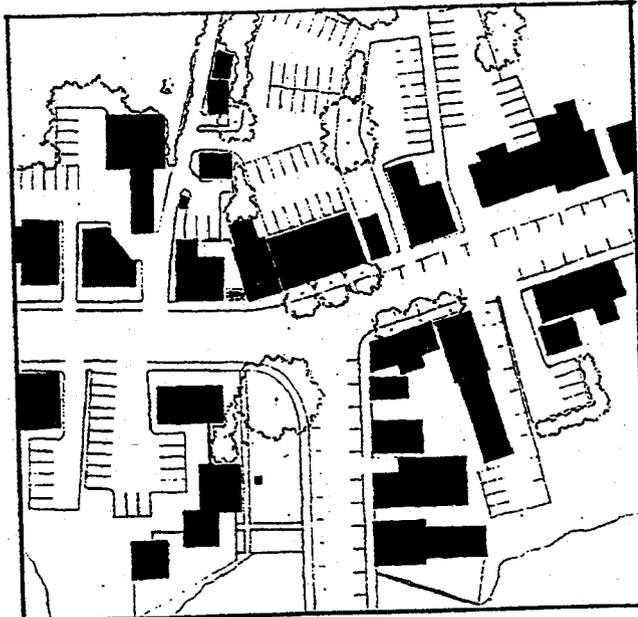


*Proposed*

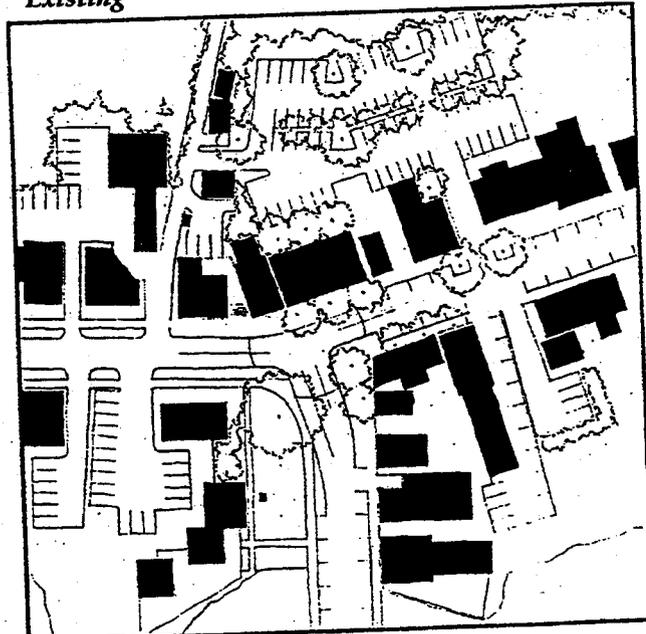
## WEST MAIN, BROWN AND MAIN STREET

*Discussion*

\* In the proposed layout, a smooth flow of cars from West Main Street onto Brown Street is aided while the widened sidewalk and tighter radius at the SE corner favors the pedestrian and slows traffic turning into Main Street. Access to and availability of parking behind the stores is clarified and simplified with connected shared parking. While the parking capacity is increased the small scale of the area is defined by additional landscaping.



*Existing*



*Proposed*

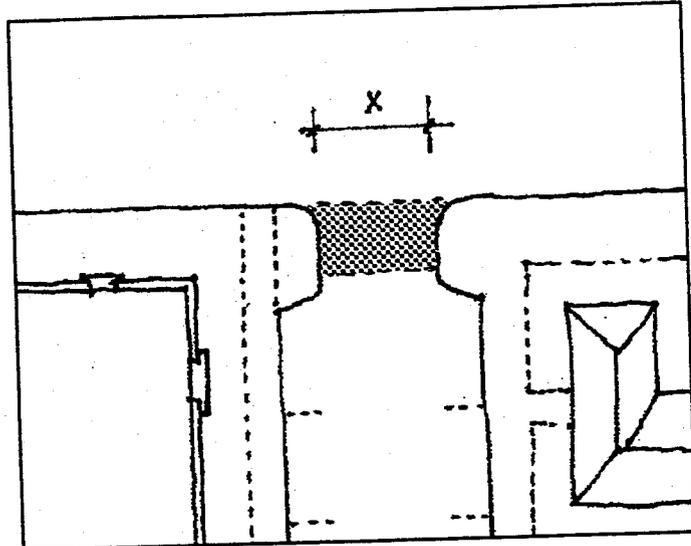
## CURB RADII

### Guideline

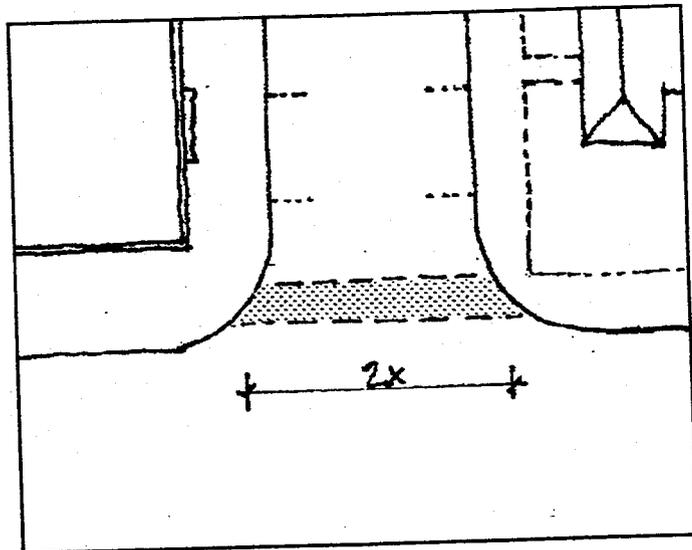
- \* Curb radii should be kept to a minimum, to reduce the time required for pedestrians to cross intersections.
- \* Crosswalks should be paved with a contrasting textured material such as brick or cobblestone and should be slightly raised in profile to slow vehicles.

### Discussion

- \* Clearly marked crosswalks and reduced pedestrian crossing time provide a safer, more hospitable pedestrian environment.
- \* Sweeping corners encourage drivers to turn corners at higher speeds. Within Wickford Village slower vehicle speeds are desirable.



**Preferred:** Extended curbs and minimal radii make street crossing easier for pedestrians.



**Discouraged:** Wide radii increase crossing distances and reduce pedestrian safety.

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Streets**

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## CURB CUTS

### *Guideline*

- \* Curb cuts and interruptions of the pedestrian sidewalk should be kept to a minimum. Cuts should be one lane wide only, and not more than 24 ft. on commercial streets, 12 ft. on residential.
- \* Where interrupted by curb cuts, the continuity of the sidewalk surface should be maintained, while the material of the driveway may be interrupted.
- \* On major roads, the number of curb cuts should be kept to a minimum.

### *Discussion*

- \* Wide curb cuts, driveway aprons and parking areas interrupt the sidewalk and destroy the scale and pedestrian continuity of streets.
- \* Frequent curb cuts on heavily travelled streets can create unsafe conditions.



***Preferred:*** Continuous sidewalks and narrow curb cuts create safer pedestrian paths.



***Discouraged:*** When the street and the parking lot are continuous, pedestrians lose their space.

## TOWN GATEWAYS

### *Guideline*

- \* Entrances to the Village should be emphasized, defined and delineated in a form that is clear and consistent with the image projected by the Village.
- \* Standard signage announcing the village should be located at each major gateway intersection.
- \* Gateway intersections should make the shift from the highway scale of major through roads to the slower more pedestrian scale of the village.

### *Discussion*

- \* Gateways leave memorable impressions on both residents and visitors. They should be maintained accordingly.
- \* Unique characteristics of the Village should be promoted by the form and imaging of each gateway.

## Design Guidelines for Streets

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### STREET LIGHTING

#### *Guideline*

\* The preferred height for street lights is 10-14 ft. Street lights should be lower than adjacent buildings and should be fitted with MH (metal halide) lamps for a blue-white light that maximizes night-time color perception and does not produce glare.

\* Lighting should be designed to light the sidewalk area and the buildings, not the street, or the sky. At intersections, however, additional lighting in the street and on crosswalks should be provided.

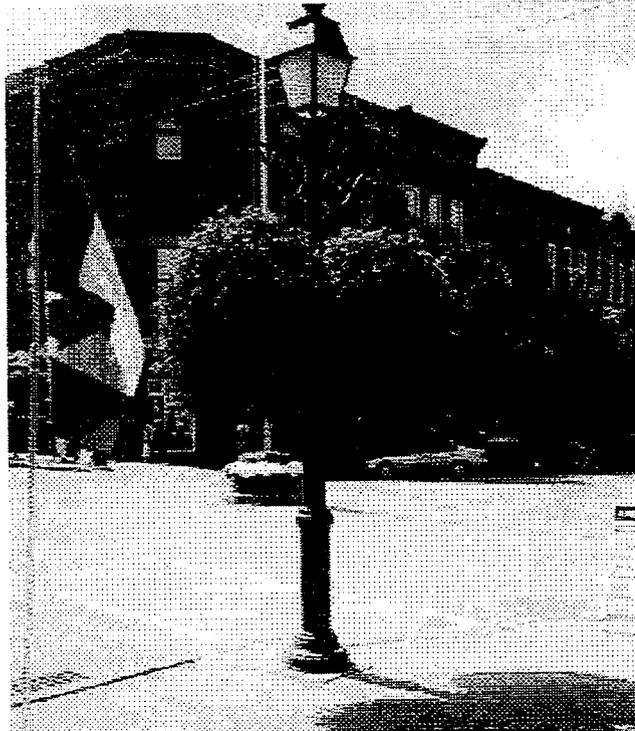
\* Cobrahead lights and unfinished metal poles are not allowed.

\* As Wickford's utilities are moved underground new street lighting may become necessary. New lighting should be simple with wooden supports designed to integrate with street signage and notice boards.

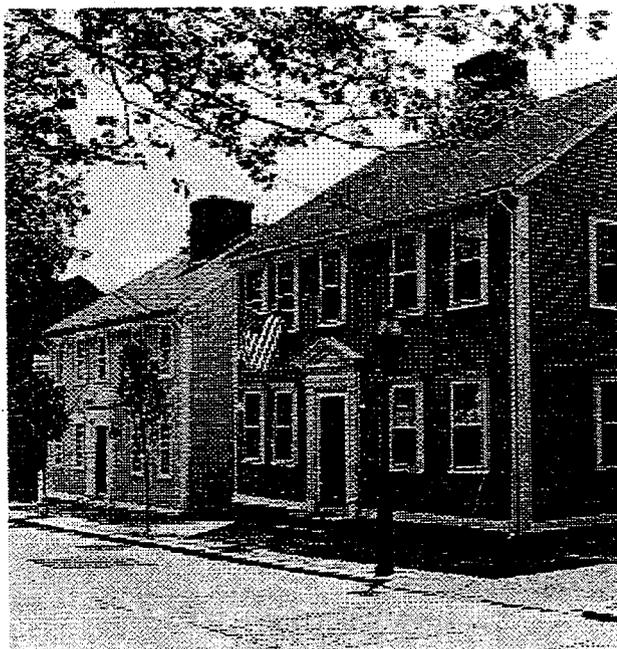
#### *Discussion*

\* Street lighting should be designed for the comfort and safety of pedestrians.

\* MH lamps are preferred over HPS (high pressure sodium) lamps which provide a yellow-orange light which distorts color perception and produces glare.



*An example of lighting the sidewalk area is illustrated above. (Photo courtesy of Kathy Romeo; Cooperstown, NY)*



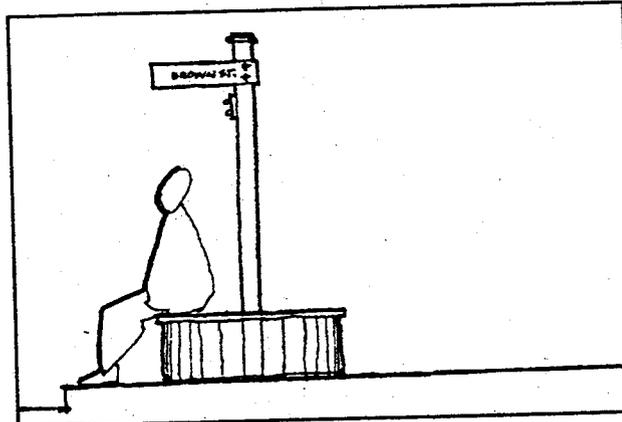
*(Photo courtesy of N. Kingstown Planning Dept.)*

GUIDELINES FOR WICKFORD VILLAGE  
Design Guidelines for Streets

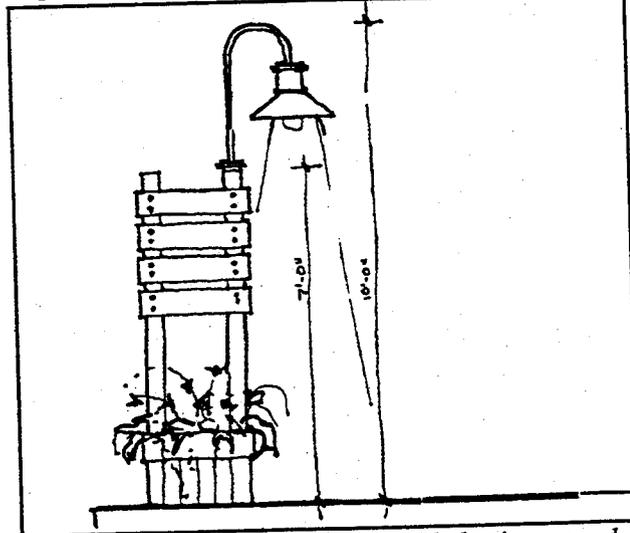
**STREET SIGNAGE**

*Guideline*

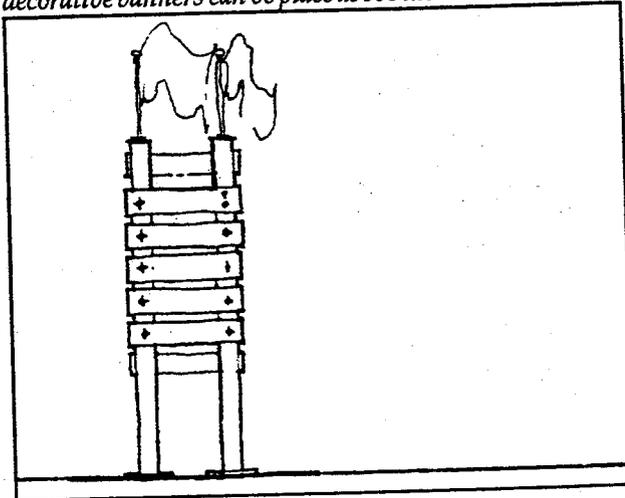
- \* Signage should be uniform and clear.
- \* Signage can be incorporated with the street lighting.
- \* Notice boards should be incorporated into the overall signage of the village in order to alleviate telephone pole postings.
- \* A distinction should be made between municipal and private enterprise signs.
- \* The marine quality of the village should be emphasized in the design of simple wood constructions as bases for directional signs, street lighting and notice boards.



*Simple wood construction can be used for street signage.*



*Street lighting can be incorporated with the signage and decorative banners can be placed above the notice board below.*



# GUIDELINES FOR WICKFORD VILLAGE

## Design Guidelines for Streets

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### PARKS

#### *Guideline*

\* Public open spaces should be visible and accessible from public streets. It should not be unusable or "residual" green space. Where possible, parks should connect to other green spaces or the waterfront. Over time, a linkage of public and private open spaces should create a "greenbelt" for the village.

\* Major open space of 10 acres or more of land left in a "natural" or uncultivated state should be made accessible with walking paths and bike trails within walking distance of homes. Active farm land should be protected.

\* Community parks may include small open greens, with paths, seating, ornamental gardens, or a tot lot playground.

\* Recreational greens appropriate for playing fields-baseball diamonds, soccer fields, tennis courts or other play spaces should be provided and maintained for the village.

\* The village center, which includes a majority of commercial and business uses should incorporate public open space in the form of small community greens or landscaped plazas.

#### *Discussion*

\* Where it is possible, every effort should be made to maintain public access as well as views to the waterfront. Emphasizing all of the natural surroundings in a community strengthen its vitality.

\* Communities benefit from accessible facilities and shared public parks.



*Open space should be visible from public streets.*



*Parks can come in many different forms. Parks provide a focus for a neighborhoods and can serve to unite new and older communities.*

## Design Guidelines for Building Lots

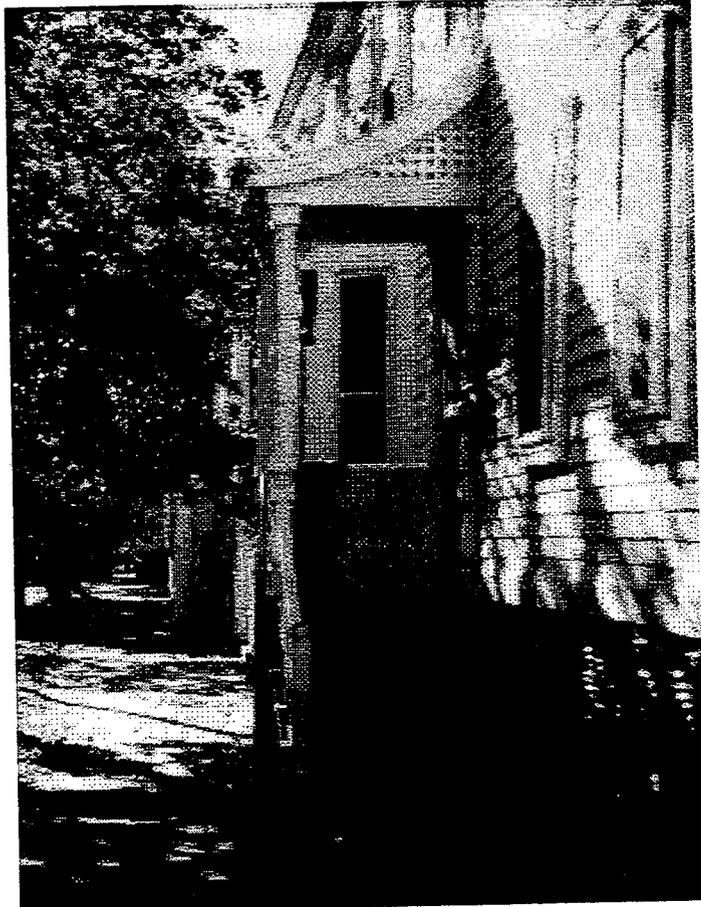
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### GENERAL NOTES FOR BUILDING LOTS

The nature and location of buildings, parking areas, and landscape features on individual lots contribute significantly to the character of the community.

These guidelines address only those aspects of site and building design which have a direct impact on the quality and character of the public space. They do not affect the interior design or use of a building or the areas of a building or site which do not face a public space.

Areas like Wickford Village develop over time in such a way as to enhance the pedestrian nature of the street, and support the distinctive character for the town.



*Private houses in Wickford align to form the streetscape.*

#### General Notes for Building Lots

Residential Lot Dimensions	3.2
Garages and Driveways	3.3
Outbuilding and Accessory Apartments	3.4
Residential Building Alignment	3.5
Building Alignment in the Village Center	3.6
Parking Lots in the Village Center	3.7
Corner Lots in Village Center	3.8
Parking Surfaces in the Village Center	3.9
Drive-throughs and Gas Stations in Village Center	3.10
Fences, Walls, and Hedges	3.11

# Design Guidelines for Building Lots

## RESIDENTIAL LOT DIMENSIONS

### Guideline

\* In new construction, single family residential lots can be as small as 5,000 sq. ft. with street frontage as low as 50 ft. if accessed by a rear alley. Without rear alley access frontage should be a minimum of 65 ft. Within the village center, street frontage should not exceed 100 ft. on the primary street.

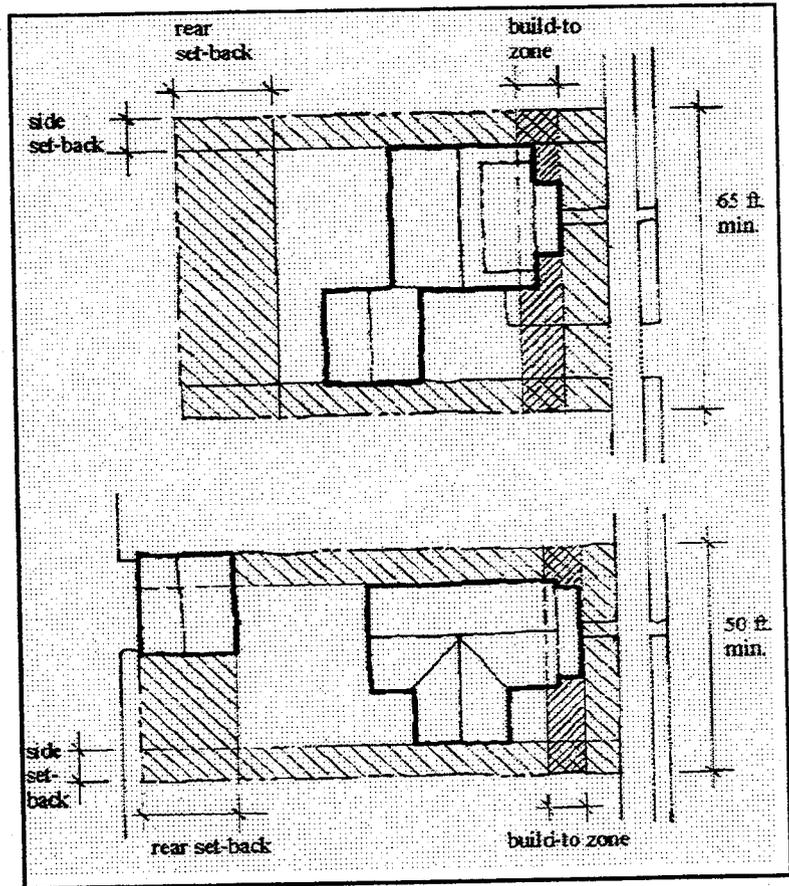
\* Minimum set backs for side yards should be 7 ft. Minimum rear yard set backs for the main house should be 20 ft.

\* Detached garages and utility outbuildings may be placed on the rear and side property lines.

\* Minimum front yard setbacks are governed by the Build-to Zone designated for each street, based on the predominant set back along that street.

### Discussion

\* Lot dimensions have a significant impact on the character of a neighborhood and should be carefully considered.



*The build-to-zone is designated based on the predominant set back along that street.*

## Design Guidelines for Building Lots

### GARAGES AND DRIVEWAYS

#### *Guideline*

\* Locate garages to the rear of houses, behind principal facades. Parking areas should be limited to the rear and side yards. Parking in the front setback is not allowed on residential streets.

\* Attached garages should have doors set a minimum of 20 ft. behind the street face of the house, unless the doors face the side. Curb cuts for residential driveways should be no wider than 12 ft.

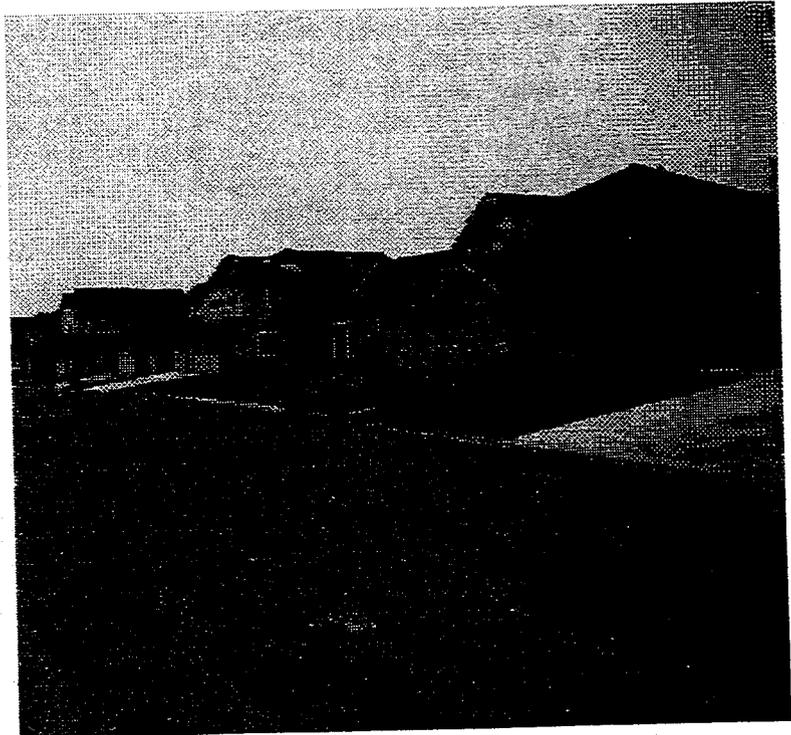
#### *Discussion*

\* Set back garages keep the street from being dominated by garage doors and curb cuts; and make narrower lots possible.

\* The character of pleasant, walkable streets can be created if garages are set back or accessed from the rear.



*Garages should be located behind principal facades of buildings and curb cuts should be narrow.*



*This street is dominated by garage doors and wide curb cuts. It is not a pedestrian friendly street.*

## Design Guidelines for Building Lots

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### OUTBUILDINGS AND ACCESSORY APARTMENTS

#### *Guideline*

\* One accessory apartment of up to 700 sq. ft. is allowed on any house lot over 6000 sq. ft. They may be located within a single family house (with a separate entry), in a separate outbuilding, or over a garage. Outbuildings should be architecturally consistent with the principal house.

\* Apartments in outbuildings must be located within the minimum side yard setback but may be located on the rear property line if there is an alley Right-of Way there.

#### *Discussion*

\* Accessory apartments increase the variety of housing options available, insuring the long-term quality and affordability of Wickford Village as a desirable place to live.



*This accessory building is architecturally consistent with the principal building.*



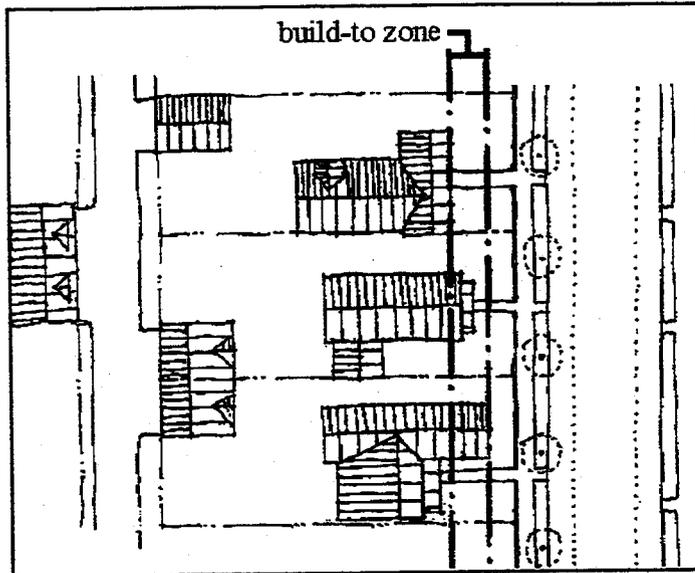
*Accessory apartments of up to 700 sq. ft. increase the variety of housing options available.*

## Design Guidelines for Building Lots

### RESIDENTIAL BUILDING ALIGNMENT

#### Guideline

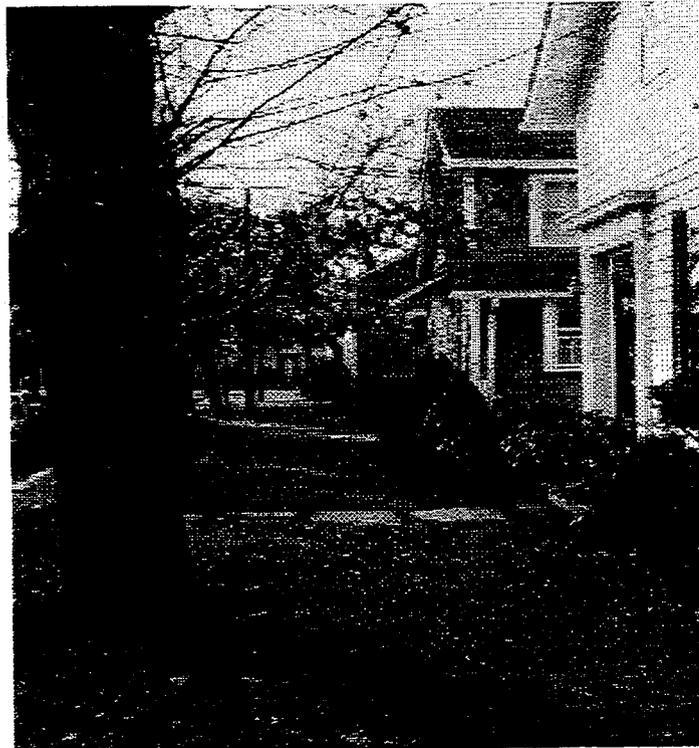
- \* Houses should align along streets to create the public space of the street.
- \* Building faces should be parallel to the street with major roof ridges either parallel or perpendicular to the street.
- \* Designate a **Build-To Zone**, beginning at not less than 5 ft. from the property line and extending not more than to 25 ft. back, on designated new streets. A minimum of 40% of the building's front wall, measured horizontally, should sit within the Build-To-Zone.
- \* Porches may encroach past the Build-To Zone.



*New street with Build-To Zone.*

#### Discussion

- \* When buildings line up along a street it creates a defined edge to the public space.
- \* A Build-To Zone in the Neighborhood Districts permits flexibility for siting buildings while maintaining the coherence of the street.



*Aligned residential buildings help define the public space.*

## Design Guidelines for Building Lots

### BUILDING ALIGNMENT IN VILLAGE CENTER

#### Guideline

\* New buildings and later additions to existing buildings should maintain the street wall.

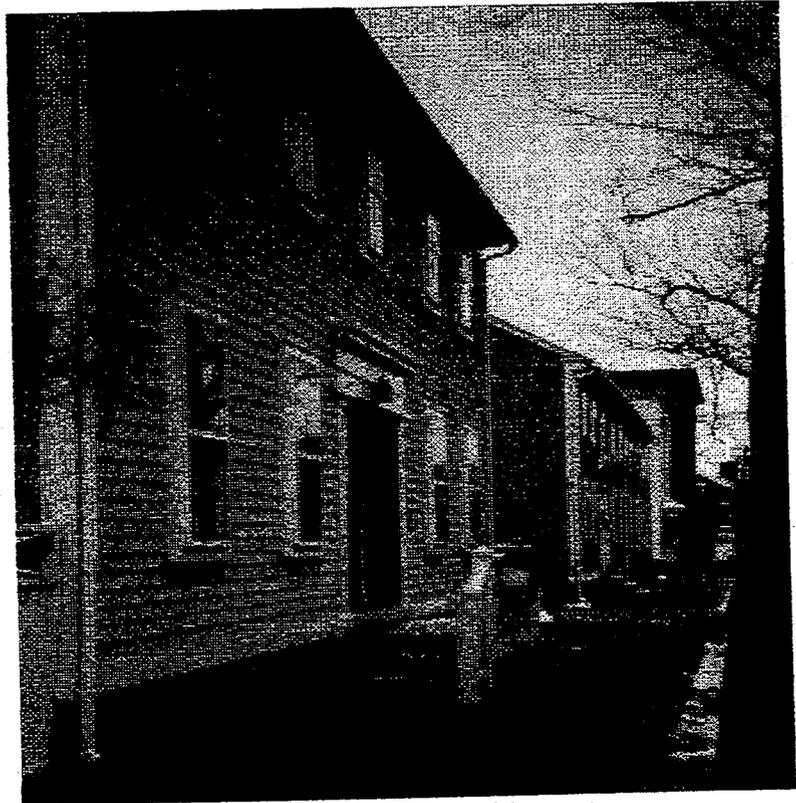
\* The **Build-To Line** in the Village Center District is the property line. The front of most buildings should sit on that line. Setbacks for up to 40% of the building front are allowed if the setback area is developed as a public porch, a plaza or garden. The area may not be used for parking. If the majority of the buildings on a block maintain the property line (constituting 70% of overall frontage), a building may be set back up to 20 ft. to create a publicly accessible plaza.

\* The preferred side yard setback is 0 ft. Side setbacks of between 0 ft. and 10 ft. are not allowed. Side yards of 10 ft. or more should be landscaped and may be used as driveways, pedestrian pathways, accessibility ramps, or semi-public spaces such as restaurant patios.

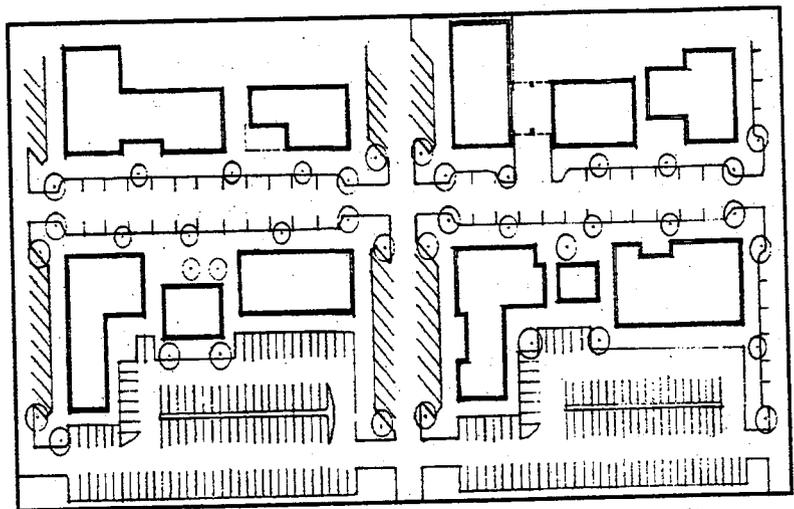
#### Discussion

\* Party wall construction is traditional on town streets.

\* Narrow side yards gaps between 0 ft. and 10 ft. are likely to be dark and dangerous and do not make good pedestrian spaces.



*Aligned buildings maintain the coherence of the street.*



*Most buildings in the village center should sit on the property line.*

## Design Guidelines for Building Lots

### PARKING LOTS IN VILLAGE CENTER

#### *Guideline*

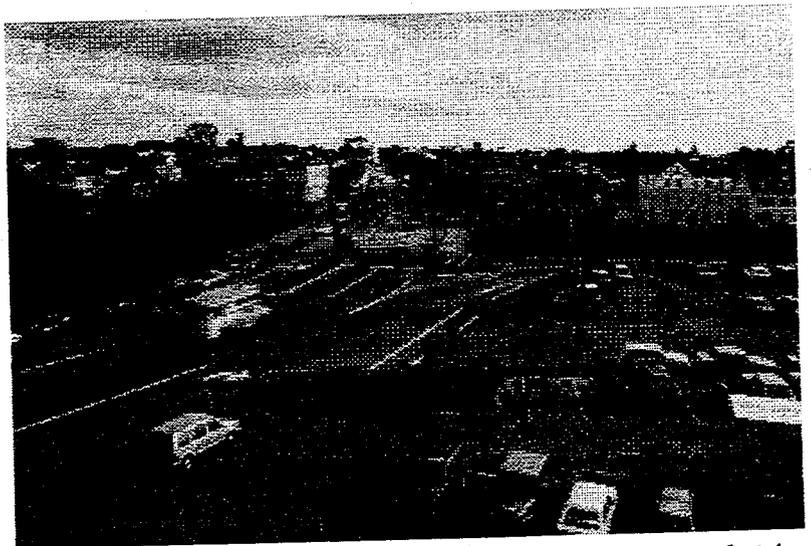
- \* Parking lots should be located to the rear of buildings or, if that is not possible, to the side with the lot screened from the street by a fence, low wall, or hedge. See *Guideline 3.10*.
- \* The paved area of a site should be no more than necessary to efficiently accommodate the required numbers of parking spaces. Additional unpaved area should be reserved wherever possible for possible peak demand use.
- \* Large parking lots should include one tree for every 8 parking spaces. An area of at least 8 sq. ft. should remain unpaved and uncompacted around each tree to allow it to grow.
- \* Clearly delineated pedestrian pathways should be provided for crossing the lot.
- \* Parking lots in side yards are discouraged. Where unavoidable they should be limited to 64 ft. in width along the street and should be screened from the sidewalk with fencing and landscaping.

#### *Discussion*

- \* Large open parking lots destroy the continuity and scale of the pedestrian environment. Parking in front of buildings disrupts the safe walking space of the pedestrian.



*Large parking lots should include one tree for every eight spaces.*



*Large open parking lots destroy the continuity and scale of the pedestrian environment.*

## Design Guidelines for Building Lots

### CORNER LOTS IN VILLAGE CENTER

#### Guideline

\* Corner parking lots are prohibited. Buildings and porches, trees, hedges, low walls and sidewalks should define the street corner. Curb cuts should be minimized and kept away from the corner.

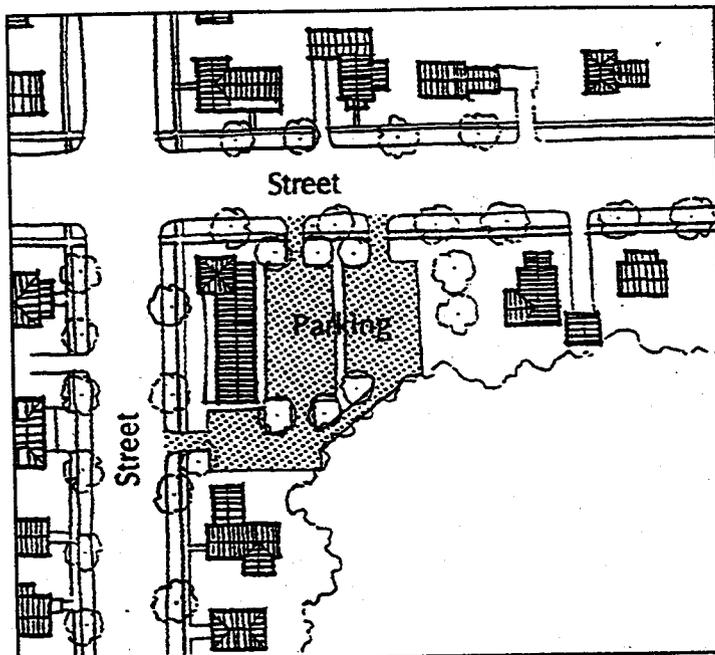
\* Parking lots should be screened from the street by buildings and vegetation. Clearly designated, safe and continuous pedestrian sidewalks and bike paths should be maintained around corners.

\* See also *Guideline 3.4* and *2.16*.

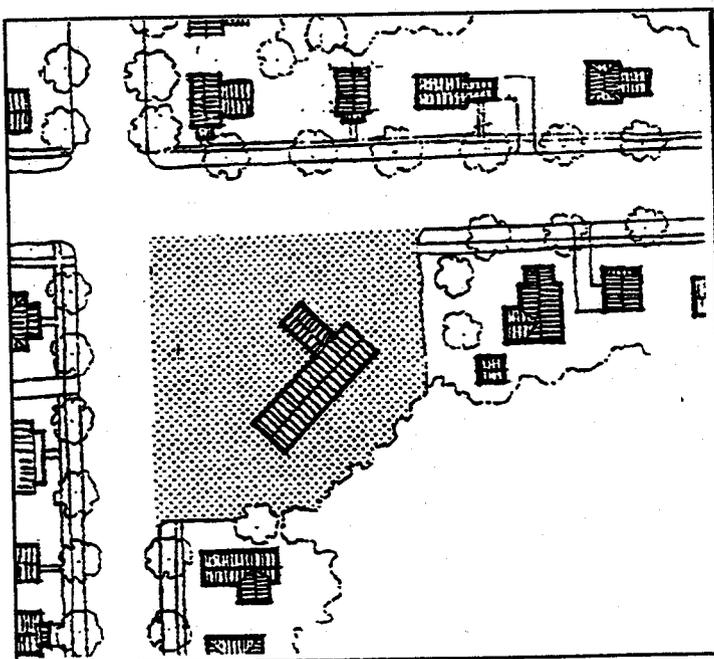
#### Discussion

\* Street corners are important points of activity and should be designed as pedestrian places.

\* Attractive intersections can encourage continuous pedestrian travel, while vacant corners discourage pedestrians from continuing to the next street.



*Preferred: Buildings should be sited to hold the corner.*



*Discouraged: Parking lots on corners are not allowed.*

## Design Guidelines for Building Lots

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### PARKING SURFACES IN VILLAGE CENTER

#### *Guideline*

- \* Parking and driveway surfaces should be permeable wherever possible.
- \* Provide on-site drainage to limit runoff.
- \* Gravel parking areas are encouraged provided they include an adequate paved walkway for wheelchair and pushcart access.
- \* Grass parking for overflow areas is encouraged.
- \* Preferred parking surface materials are gravel and other porous pavement and pavers. Impermeable asphalt surfacing may be used for the travel lanes in a lot but is discouraged for the actual parking spaces.



*Differentiated and permeable parking surfaces improve the appearance and performance of parking areas.*

#### *Discussion*

- \* Permeable surfaces and on site drainage reduce the need for storm drainage infrastructure and allows storm water purification through the soil, reducing pollution of ground water.

## Design Guidelines for Building Lots

### DRIVE-THROUGHS AND GAS STATIONS IN VILLAGE CENTER

#### Guideline

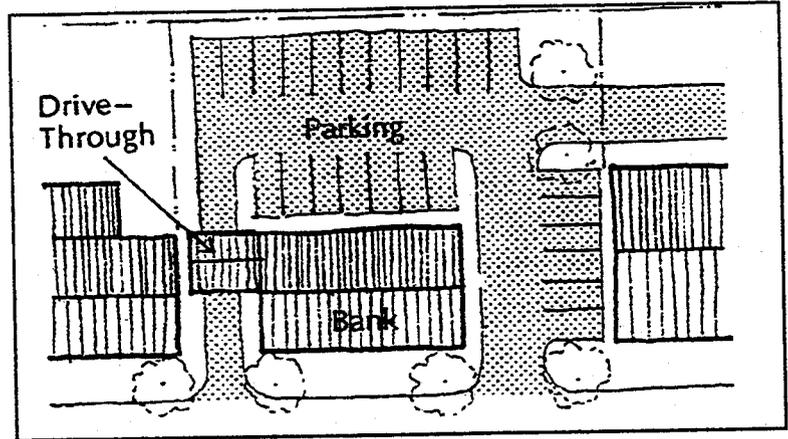
\* Gas stations and drive-throughs should conform to building alignment, parking lot and curb cut requirements. These uses should avoid corner sites and maintain clearly designated, safe and continuous pedestrian sidewalks and bike paths.

\* Drive-through canopies should be scaled to passenger automobiles and not to buses or large trucks. Canopy heights should not exceed 12'-0" clear.

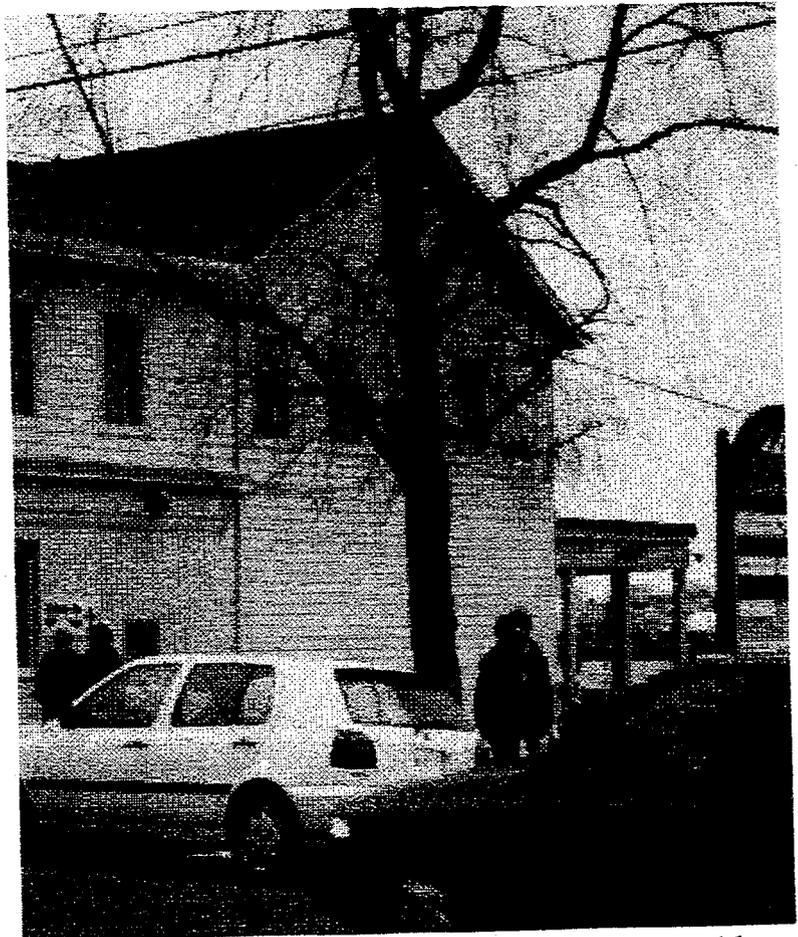
\* Locate the drive-through to the rear of buildings wherever possible. Narrow curb cuts are required. See *Guideline 2.16*.

#### Discussion

\* Drive-throughs and gas stations can be convenient for cars without ruining the pedestrian scale. Intersections with more than one drive-through corner are destructive of the town character and pedestrian scale.



*Drive-through buildings must conform to guidelines.*



*This drive-through in Wickford Village is convenient for cars and does not ruin the pedestrian scale of Brown St.*

## Design Guidelines for Building Lots

### FENCES, WALLS AND HEDGES

#### Guideline

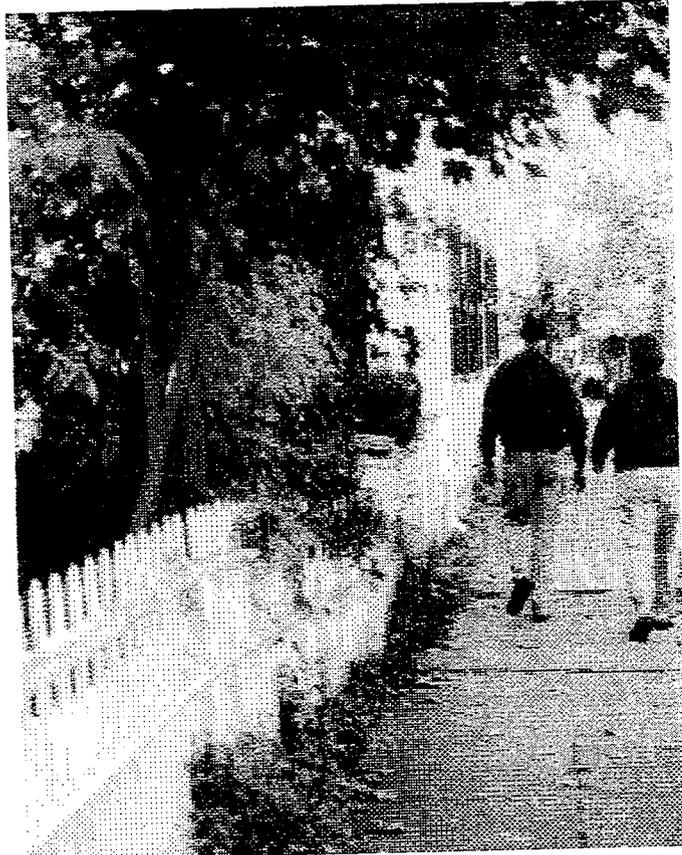
\* Low fences, walls and hedges (less than 4 ft. high) are encouraged along front property lines, especially where the continuity of the street is disrupted by a vacant lot, a parking lot, or a building set back further than the norm.

\* Tall chain link fences, high solid fences and high hedges are not allowed along the front property line.

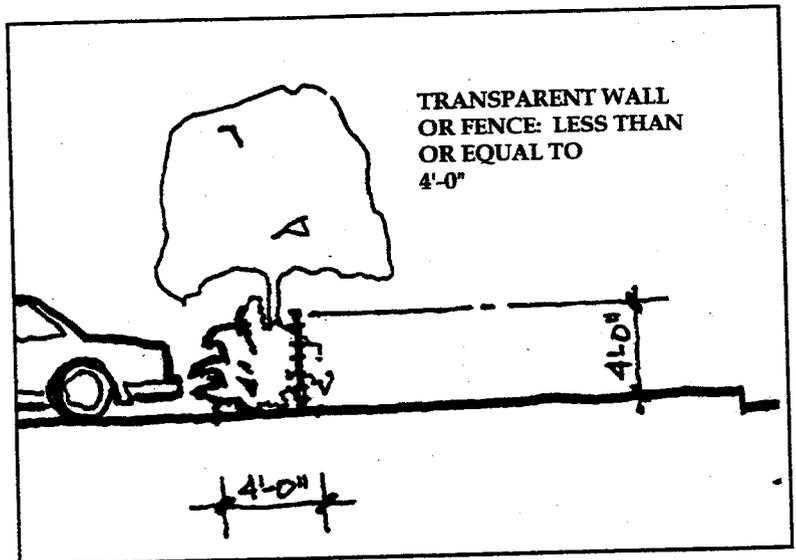
#### Discussion

\* Low walls, fences, and hedges define walkways, give pedestrian scale to the street, and maintain the historic Rhode Island character.

\* Tall fences and hedges create unfriendly barriers.



*Pedestrian space clearly defined in a neighborly manner*



*Fences can define the public and private spaces of the street.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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## GENERAL NOTES FOR BUILDINGS

These guidelines address those aspects of buildings design which have a direct impact on the quality and character of the public space. They do not affect the interior design or use of a building or the areas of a building or site which do not face a public space.

These guidelines apply to the initial design and construction of new buildings in Wickford Village and to substantial renovations and additions to existing buildings.

Areas like the village of Wickford can develop over time in such a way as to support and enhance the pedestrian nature of the street, and preserve the distinctive character for the town.



### General Notes for Buildings

Street Walls	4.2
Dwelling Entries	4.3
Storefronts	4.4
Flood Plain Design	4.5
Upper Stories	4.6
Balconies and Bay Windows	4.7
Awnings and Overhangs	4.8
Porches	4.9
Building Signage	4.10
"Trademark" Buildings	4.11
Environmental Design	4.12
Building Materials	4.13

# GUIDELINES FOR WICKFORD VILLAGE

## Design Guidelines for Buildings

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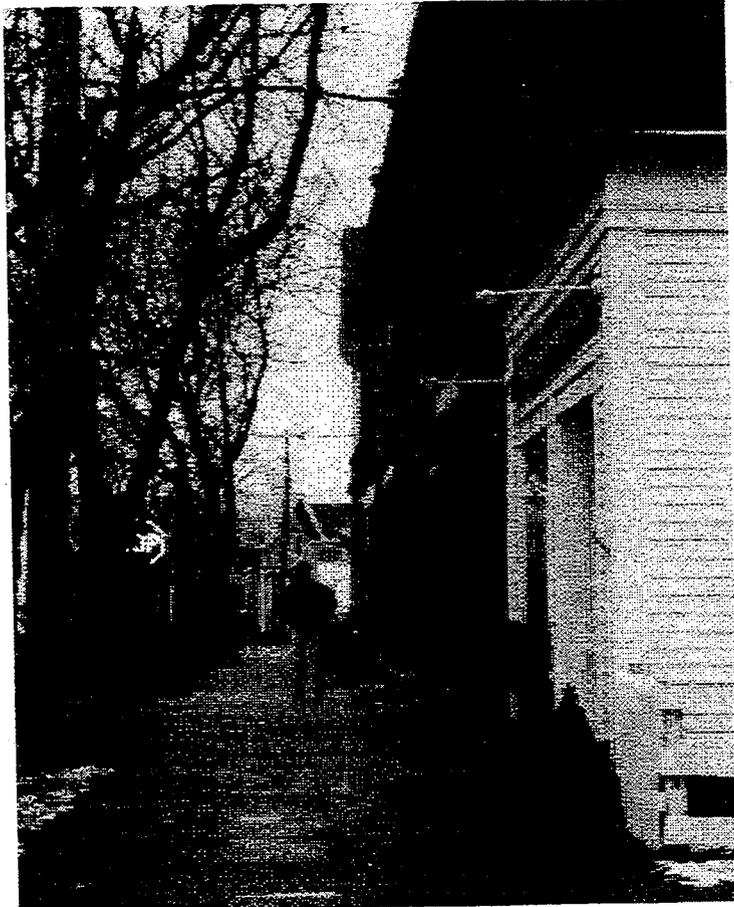
### STREET WALLS

#### *Guideline*

- \* In Wickford Village, a variety of building styles and sizes combine to create a dynamic streetscape. On designated streets, the front facade of buildings must be a minimum of 14 ft. tall measured from the sidewalk. Two-story buildings are encouraged and tall single story uses are allowed. False front facades may be utilized to achieve the required height.
- \* Building foundations may be exposed if the first floor is elevated above street level. Exposed foundations must be faced with brick, stone, or stuccoed.
- \* The building wall should finish at the roof in a parapet with a cornice or in an elaborated overhang.
- \* The roofs of non-residential buildings should be simple, with the principal gable running parallel or perpendicular to the street.
- \* Entry to the first public level must accommodate wheeled access.

#### *Discussion*

- \* A taller building wall is desirable for the spatial definition of the street. Tall storefronts give a more open feel to the pedestrian space. The extra height at the first story provides a good surface for signage.
- \* Upper story uses are encouraged to increase the vitality of the street.



*Taller building walls define the space of the street.*



*Tall single story buildings are allowed. Signs and awnings can be used to maintain the vitality of the street wall.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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## DWELLING ENTRIES

### *Guideline*

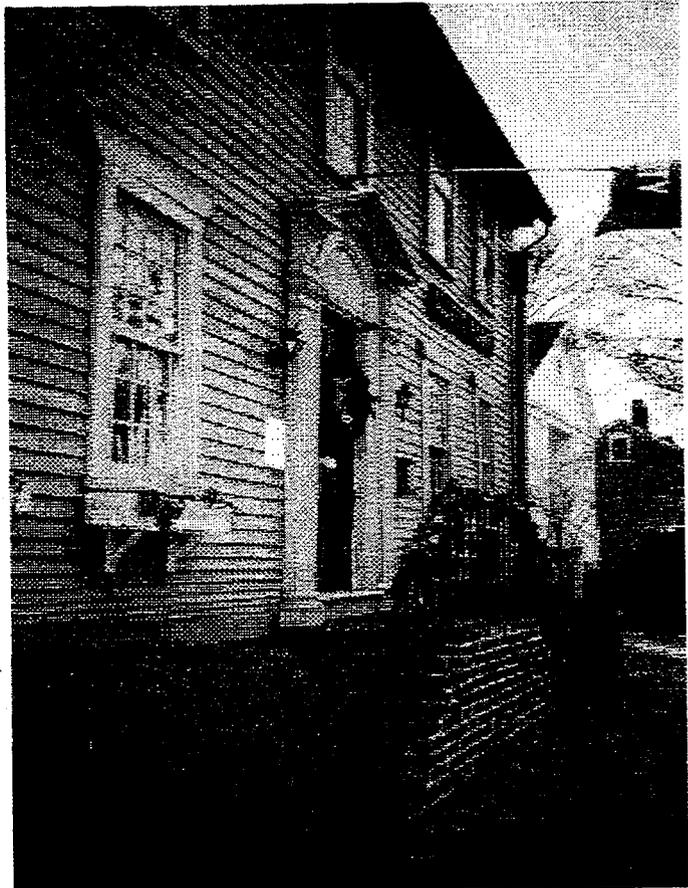
- \* Principal entries to dwelling units should be oriented to and visible from the street. Single family houses should have front doors facing the street. In multi-family house, entries should front on streets or side yards rather than parking lots or interior courts. Entries from courtyards which face the street are allowed. Blank walls facing the street are not allowed.
- \* Apartments over stores along main streets should have lobby access to the street. Secondary entries may face rear lots.
- \* One accessory unit per house lot may be accessed from a rear alley or side yard.

### *Discussion*

- \* Entries facing the street enliven the public space of the street. Outdoor spaces are safer when overlooked by windows and entries.
- \* Apartment buildings and townhouses facing parking lots destroy the pedestrian scale of the village.



*A large detached multi-family house with multiple building entries.*



*Entries facing the street enliven the public space of the street.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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## STOREFRONTS

### *Guideline*

\* On designated streets, buildings should have glazed storefronts at street level to encourage pedestrian traffic and commercial activity. Transparency along the street wall at the first floor should be at least 60% of the wall area over a 7 ft. high zone between the heights of 2 and 9 ft. from the sidewalk. Windows at street level should provide views of interior spaces or displays and not be obscured by signage.

\* No first floor of a new commercial building facing street in the Village Center should have more than 10 linear ft. of windowless wall. No building should extend along the street for more than 35 linear ft. without an entrance.

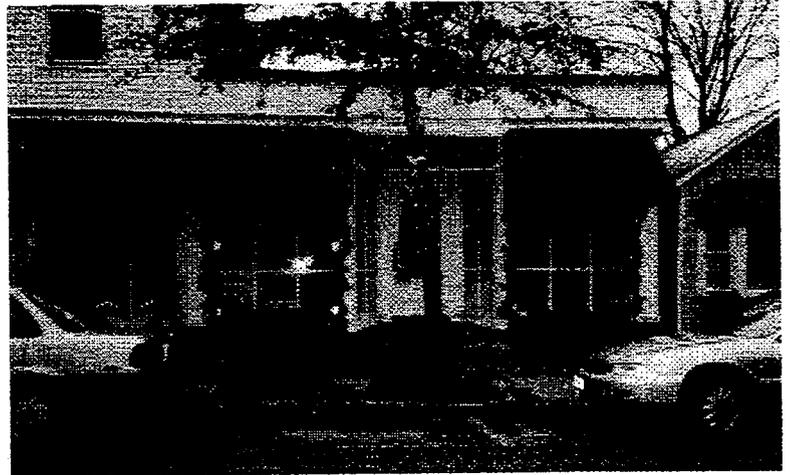
\* Heavily tinted or reflective glass is not allowed. Storefronts must have the capacity to be naturally ventilated.

### *Discussion*

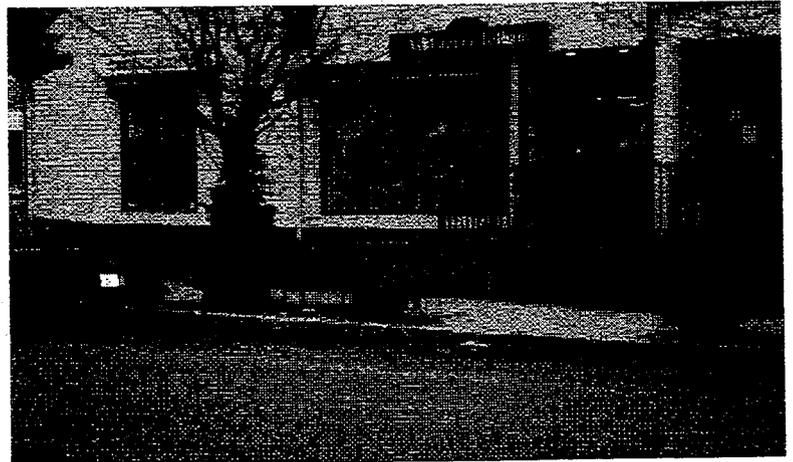
\* Storefront windows and doors are essential to a successful pedestrian shopping street. They provide access, visual interest and light onto the sidewalk.

\* Blank walls and small, high windows create an unfriendly streetscape and discourage pedestrian travel.

\* Windows are the key visual element for establishing and maintaining the architectural character of the village.



*Windows are the key visual element for establishing and maintaining the architectural character of the village.*



*Windows at street level provide views of interior spaces, access, visual interest and light onto the sidewalk.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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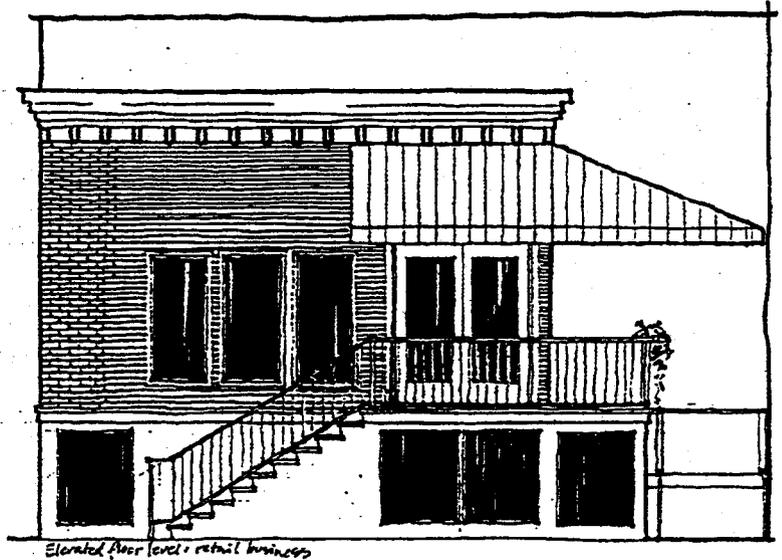
## FLOOD PLAIN DESIGN

### *Guideline*

- \* When building in a designated flood plain, the floor level of retail businesses would be above current street level.
- \* Accessibility issues should be handled in the side set back which shall be 10 feet wide on one side. Ramp access should be shared whenever possible.
- \* Entry steps should be parallel to the sidewalk. Attractive stairways leading to ample and visually open landings will help keep shop entries visible. Closed railings for stairs and landings are discouraged.
- \* To avoid blank walls at the pedestrian level, high basements should be animated with display windows.
- \* Storefront guidelines still apply to the new first floor level.

### *Discussion*

- \* In the event that the Town of North Kingstown adopts a flood plain zone, much of Wickford Village would be in this zone.
- \* In order to preserve and maintain the pedestrian character of the street, new businesses with raised floor levels must provide for pedestrian access and visual interest at street level.



*Elevated floor level: retail business*

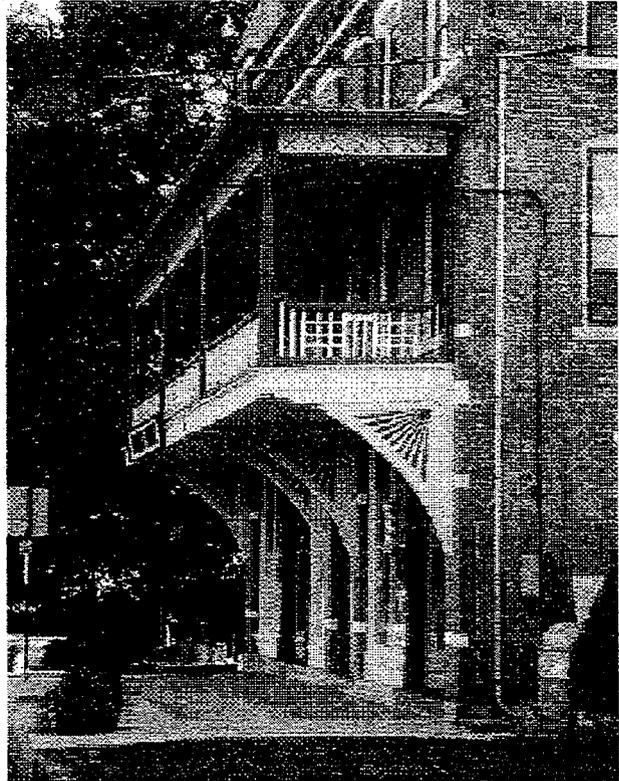
## BALCONIES AND BAY WINDOWS

### *Guideline*

\* Occupiable balconies and bay windows are encouraged and may project over the sidewalk up to 3 ft. with 10 ft. clearance to the sidewalk.

### *Discussion*

\* Bay windows and balconies enliven the street by creating interaction between upper story uses and the street.



*Balconies enliven the street by creating interaction between upper story uses and the street.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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## AWNINGS AND OVERHANGS

### *Guideline*

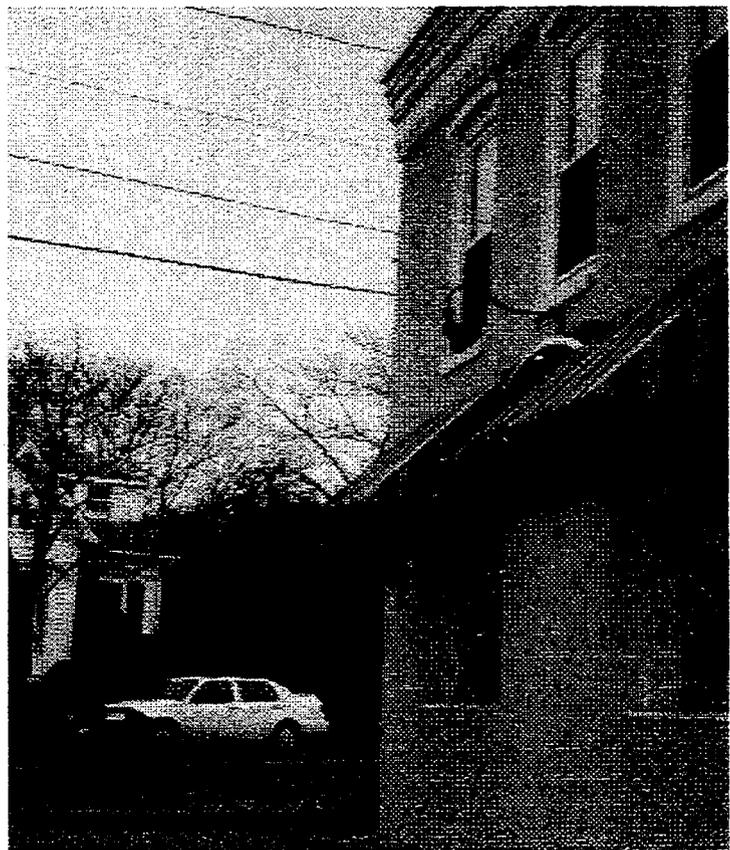
- \* Porches, awnings, arcades and overhangs are encouraged to provide a convenient covered sidewalk zone for pedestrians.
- \* Awnings are retractable fabric covered frames and may extend up to 6 ft. over the sidewalk with a minimum of 9 ft. clear underneath them.
- \* Overhangs may be like awnings but are of fixed construction, generally with a metal standing seam or corrugated surface. They may also be roofed to match the building's principal roof.

### Discussion

- \* On mixed-use streets awnings and overhangs offer weather protection and an intimate scale for pedestrian activity.



*Awnings provide convenient pedestrian zones on mixed-use streets. (Photo courtesy of Dunn Associates)*



*Awnings over individual windows add interest to the streetscape.*

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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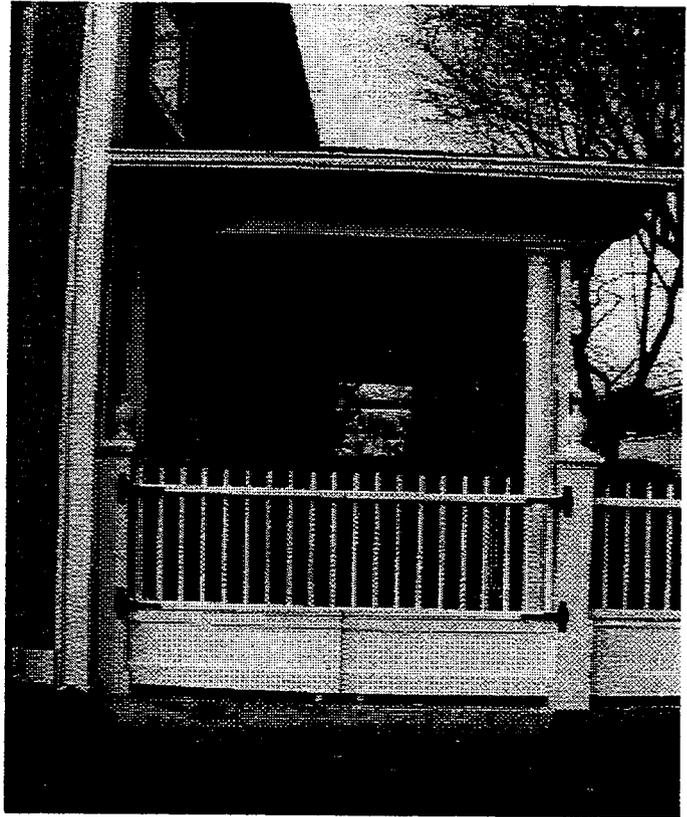
## PORCHES

### *Guideline*

- \* Front and side porches on residential and mixed-use buildings are encouraged.
- \* The depth of porches should be between 4 ft. and 10 ft.
- \* Square porch posts must be a minimum of 5 1/2 inches wide. Turned columns must not be narrower than 4 inches at any point.

### *Discussion*

- \* Front porches on houses create a transitional space between the public realm of the street and the private realm of the interior which encourages socializing along the street and adds architectural interest for the pedestrian.
- \* On mixed-use streets porches offer weather protection and an intimate scale for pedestrian activity.
- \* Double porches and porches which meet the sidewalk are a feature of many commercial and civic buildings in Rhode Island towns.



*Front porches on house create a transitional space between the public realm of the street and the private realm of the interior.*

## GUIDELINES FOR WICKFORD VILLAGE

# Design Guidelines for Buildings

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### BUILDING SIGNAGE

#### *Guideline*

- \* The maximum total area of signage for a facade should not exceed 3 sq. ft. for 1 linear ft. of building frontage. Such signs may not be externally illuminated and should be located on the buildings so as not to obscure architectural features and detail.
- \* However, one sign per business may be internally or back lit if located within a "signage band" from 9 to 12 ft. from the sidewalk. Mural signs painted on buildings are not limited in size unless they are lit.
- \* Signs should be designed, grouped and organized so as to be recognizable, legible and uncluttered in appearance.
- \* Billboards and roof signs are not allowed.
- \* Multiple advertising signs in windows are discouraged and should not obscure views into storefronts or displays.

#### Discussion

- \* The architectural elements of a building (such as side walls, parapets, and awnings) may effectively be utilized to carry appropriate signage without the need for bulky freestanding and billboard type signs.



*When appropriately designed, building walls can be utilized to carry appropriate signage.*

**ATTACHMENT #3  
WICKFORD VILLAGE  
PARKING INVENTORY**

## **Appendix: New Street Trees**

Wickford has room to plant many new street trees especially along West Main Street and Phillips Street. Brown Street has a need for trees along the east side of the street. Unfortunately, the eastern side of Brown Street does not have a planting strip like the western side. Cutting a three foot hole in the side walk and planting a shade tree in such a small space is not the best method for new tree plantings. Areas of high pedestrian traffic like Brown Street, create a more difficult and costly situation. A solution would be a continuous four foot strip of turf and trees in areas of low pedestrian traffic. The majority of the Brown Street could use a four foot continuous strip of brick, or stone, laid over a sand base which would create a permeable surface to allow air and water to reach the tree roots. Trees should be planted in loam that is at least two feet deep by four foot square.

Trees growing under utility lines will always be misshaped from line pruning. Underground utilities would prevent this kind of damage. When underground utilities are installed around existing trees, root damage can be quite severe.

In many areas along Main Street and Brown Streets buildings are very close to the street. There is not enough room for large shade trees. Small to medium, or narrow growing trees would be the best choice for these areas.

320 Phillips St.

Norway ●

294 Phillips St.

(Dying) Elm ●

256 Phillips St.

Norway ●

Honeylocust ●

Honeylocust ●

Honeylocust ●

Honeylocust ●

Loop Drive

### Legend

Elm - *Ulmus americana*

Honeylocust - *Gleditsia triacanthus*

Norway - *Acer platanoides*

Phillips Street  
from Tower Hill  
Road to Loop  
Drive  
no scale

Boone Street

Hickory ●

● Norway

● Norway

● Norway

● Norway

Honeylocust ●

● Norway

110 Phillips

● Norway

● Norway

Elm ●

● Norway

Wickford  
Elementary  
School

stairs

● Norway

Norway ●

● Norway

● Norway

● Norway

Phillips Street  
from Boone  
Street to  
Brown Street  
no scale



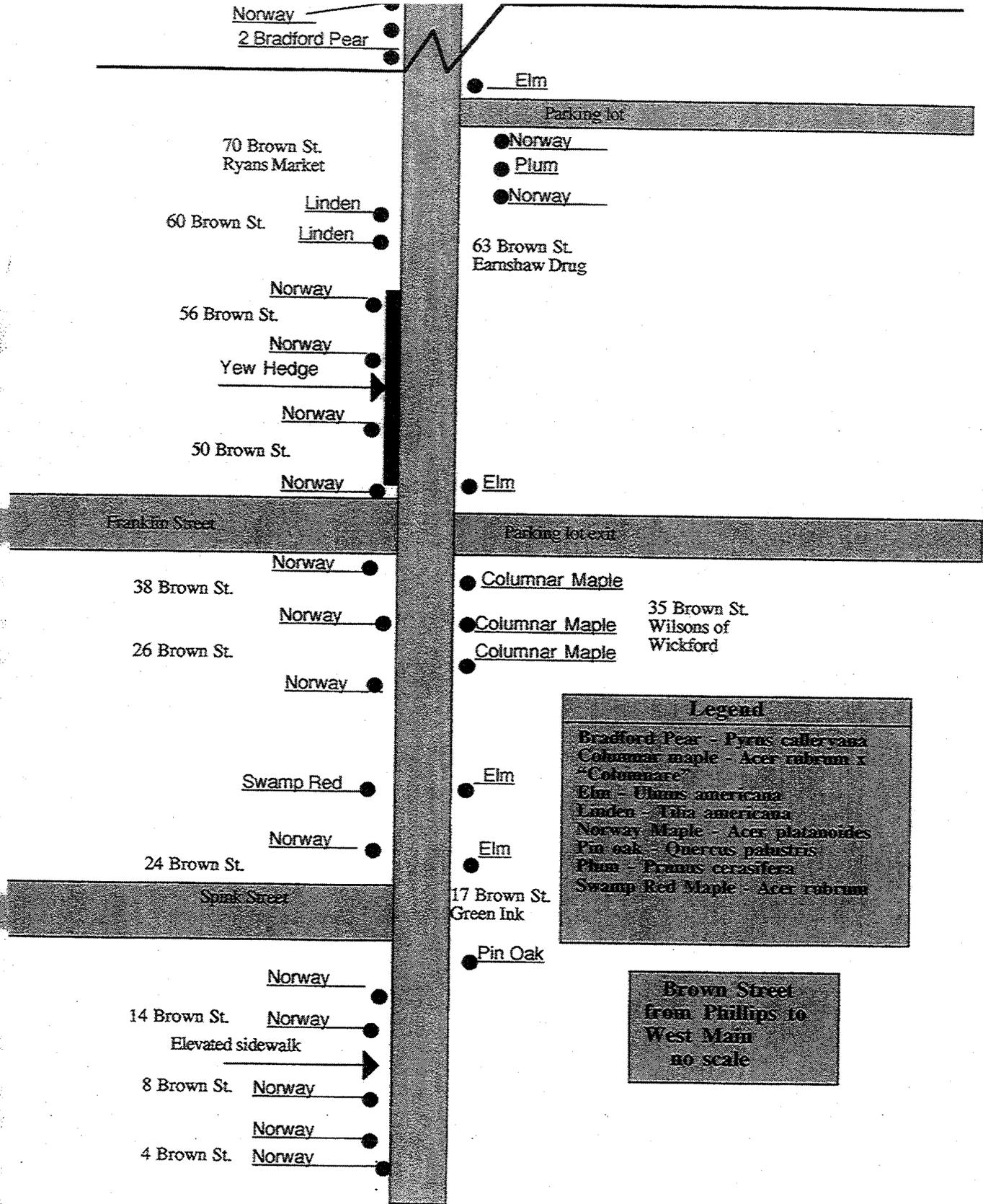
60 Phillips

Oak ●

**Legend**  
Elm - *Ulmus americana*  
Hickory - *Carya glabra*  
Honeylocust - *Gleditsia*  
*triacanthos* x  
Norway - *Acer platanoides*  
Oak - *Quercus rubra*

Norway ●

30 Phillips



**Legend**

Bradford Pear - *Pyrus calleryana*  
 Columnar maple - *Acer rubrum* x "Columnare"  
 Elm - *Ulmus americana*  
 Linden - *Tilia americana*  
 Norway Maple - *Acer platanoides*  
 Pin oak - *Quercus palustris*  
 Plum - *Prunus cerasifera*  
 Swamp Red Maple - *Acer rubrum*

Brown Street  
 from Phillips to  
 West Main  
 no scale



Phillips Street, like West Main has only a scattering of street trees. There are a few elm trees, one that is dead, a few honey locust, and at Wickford Elementary, 12 Norway maples that were severely pruned for utility maintenance.

In many places there is substantial room for new plantings of shade trees. Again, only one species should be planted along the entire street. I would recommend one of the following: *Acer rubrum* 'Scarlet Sentinel', Columnar Swamp Red Maple; *Zelkova serrata*, Japanese Zelkova; or *Ginkgo biloba*, Ginkgo male form.



166 Main

Norway ●

Norway ●

Beech ●

Norway ●

### Legend

- Ash - Fraxinus americana
- Beech - Fagus sylvatica
- Dogwood - Cornus florida
- Honeylocust - Gleditsia triacanthos x
- Hybrid Elm - Ulmus x
- Linden - Tilia americana
- Norway Maple - Acer platanoides
- Pin Oak - Quercus palustris
- Plum - Prunus cerasifera
- Swamp Red maple - Acer rubrum

Norway ●

161 Main St.

Norway ●

151 Main St.

Plum ●

146 Main St.

Hybrid Elm ●

Hybrid Elm ●

145 Main St.

Norway ●

143 Main St.

Norway (Dying) ●

Pleasant St.

Bay St.

134 Main St.

Linden ●

Linden ●

Norway ●

126 Main St.

Norway ●

Dogwood ●

Norway ●

110 Main St.

Norway ●

Ash ●

Norway ●

125 Main St.

Norway ●

121 main St.

Norway ●

Norway ●

Norway ●

Norway ●

109 main St.

Main Street from Wall Street to the Town Dock

Side walk damage from tree roots

Fowler St.

Fountain St.

Pin Oak ●

Norway ●

94 Main St.

Swamp Red ●

(Dying)

Norway ●

84 Main St.

Linden ●

Norway ●

Linden ●

Norway ●

Linden ●

85 Main St

Wall St.

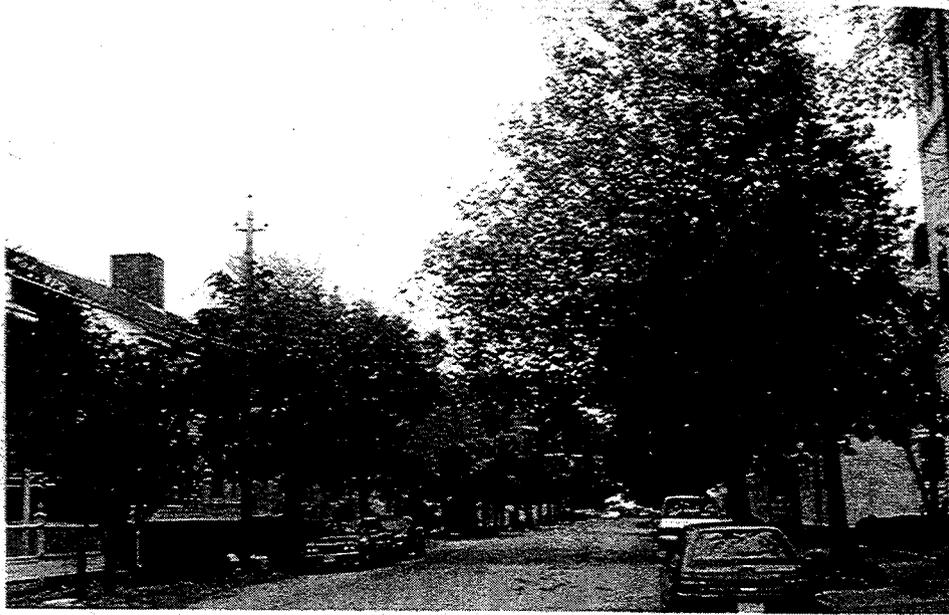
Side walk damage from tree roots



Brown Street has quite a few street trees that are large and in fair condition. There are 16 Norway maples, the 3 located at 4 Brown St. are in very poor condition, two formidable Linden trees at 60 Brown St.; one Swamp maple, three Elm, one Pin Oak, one Plum, three young Columnar Maples at Wilsons of Wickford, and two newly planted Bradford Pears at the park.

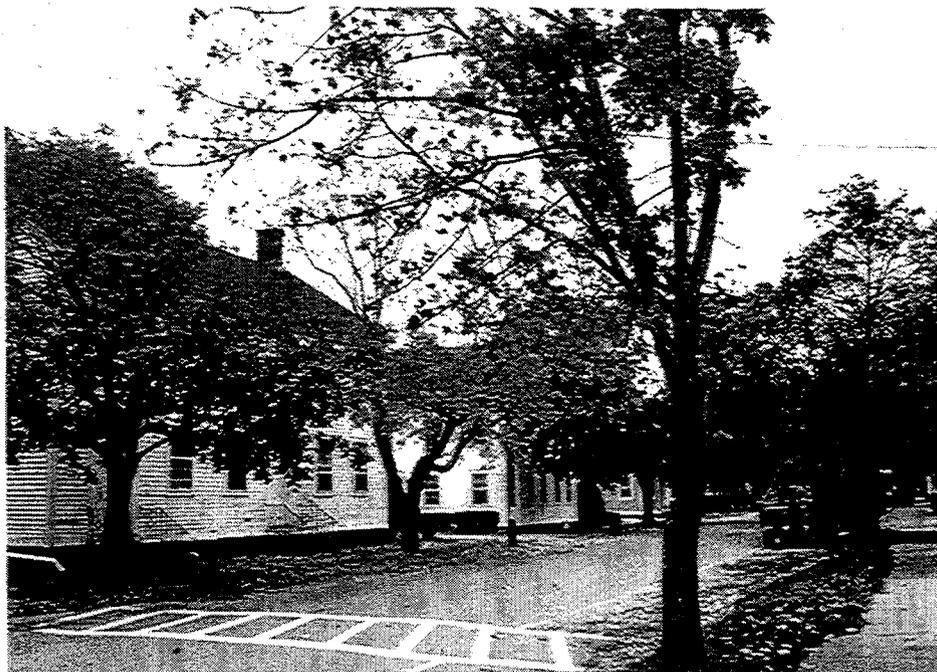
Norway Maples are a poor choice for a street tree and over time should be replaced. New Brown Street trees will have a difficult time growing in the limited space available. I recommend that new trees be of a smaller nature than the existing trees such as *Acer campestre*, Hedge Maple; *Carpinus betulus*, European Hornbeam; or *Syringa reticulata*, Japanese Tree Lilac. Brown St. like all the major streets of Wickford, should be planted with a single species for a harmonizing effect.





Main St. is heavily planted with disease prone Norway Maples . There are a few large vigorously growing London Planetrees that are destroying the sidewalks. Some of the Ash trees at 16 Main St. are dead while others in the area are vigorously growing. There are a few Swamp Red Maples, four stately Lindens, some Hawthorns, a Dogwood , a scattering of Bradford Pears, and even a misplaced Beech.

Main St. is blessed with a planting strip between the road and the walk enabling trees that make poor street trees survive and in some cases flourish. I would recommend one of the following species as replacement trees: *Acer rubrum* 'Scarlet Sentinel'; Columnar Swamp Red Maple; or *Zelkova serrata* , Japanese Zelkova.



Norway

wall st.

### Legend

- Ash - *Fraxinus americana*
- Bradford Pear - *Pyrus calleryana*
- Crabapple - *Malus x*
- Hawthorn - *Crataegus x*
- London plane - *Platanus acerifolia*
- Norway Maple - *Acer platanoides*
- Pin Oak - *Quercus palustris*
- Swamp Red Maple - *Acer rubrum*



Norway  
76 Main St.  
Norway

Norway  
71 Main St.

Swamp Red  
64 Main St.

Norway

Norway

61 Main St.

London



Hawthorn  
56 Main St.

St. Pauls Church

Hawthorn

Pin Oak

Church St.

Gold St.

Main Street  
from Brown  
Street to Wall  
Street  
no scale

Norway  
Norway  
First Baptist Chapel

Norway  
41 Main St.

Norway  
Norway  
First Baptist church

Norway  
Norway  
31 Main St.  
3 Norway (new and small)

24 Main St. Ash

London

4 Bradford Pears

London 25 Main St.

16 Main St. London

Ash

Ash  
Ash

B. Pear  
Ash (dying) 15 Main St.

2 Ash

B. Pear 7 Main st.

Crabapple

B. Pear 1 Main St.

9 Main St. 2 Ash

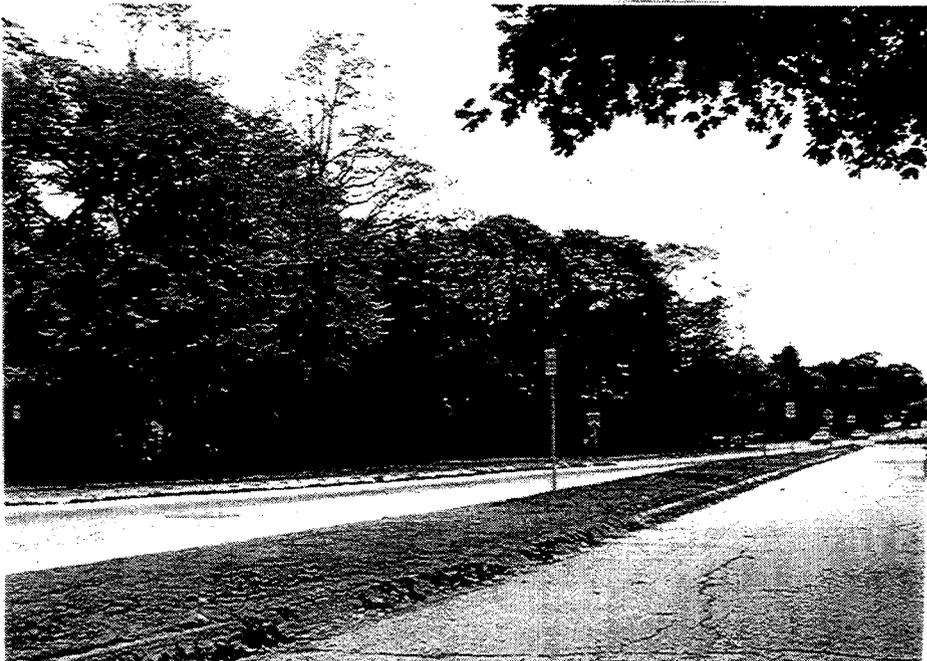




West Main Street has only a few street trees . There is ample room for many new trees that could be planted along the entire length of West Main .

All new trees should be selected from the "*Sustainable Trees And Shrubs For Southern New England*". Only one species should be planted along the entire street.

My first tree choice would be a cultivar selection of *Magnolia x loebneri* . Loebner Magnolia, is a very adaptable tree to extremes in soil and sun requirements. It would be a wonderful sight to see West Main come alive each spring with a profusion of blooms. Young, 1 inch caliber, trees from a whole sale nursery can be purchased at a very reasonable cost. Most large nurseries require minimum orders . Residents living along West Main could be encouraged to purchase trees at cost from the town.



West  
Main St.

126 West Main St.

Norway Maple ●

120 West Main St.

● Norway Maple

115 West Main St.

● Norway Maple

● Norway Maple

● Linden

● Pin Oak

83 West Main St.

Norway Maple ●

Newton Ave.

**Legend**

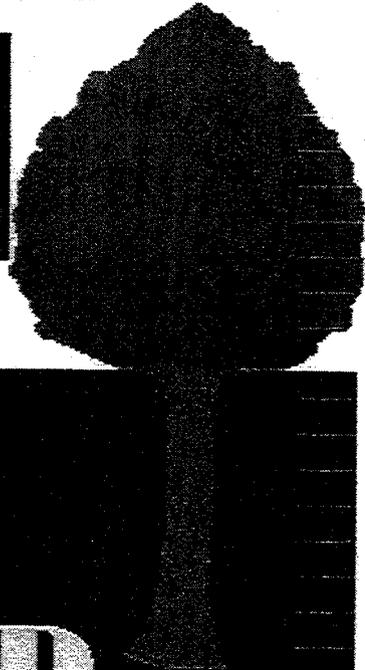
- Linden - *Tilia americana*
- Norway Maple - *Acer platanoides*
- Pin Oak - *Quercus palustris*

West Main Street starting at  
Newton Ave.  
no scale

**THE TREE**

**SURVEY**

**OF  
WICKFORD**



**By J. Peter Morgan  
North Kingstown Tree Warden**

**ATTACHMENT #2**  
**THE TREE SURVEY OF WICKFORD**

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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## **BUILDING MATERIALS**

### *Guideline*

\* Preferred building materials are wood, brick and stone. Preferred finish treatments are stain, paint, stucco, or unfinished brick or stone. Use of concrete and concrete block is acceptable below the first story. The use of corrugated concrete and "cinderblock" walls as the finish surface is not permitted. Tile and metal wall surfaces are not typical materials in this area, but may be acceptable in limited applications. The use of vinyl siding is discouraged unless detailed to be consistent with traditional wood construction and trim details. Asphalt and asbestos wall surfaces are prohibited.

\* The preferred roofing materials are standing seam metal. Slate, wood and asphalt shingles is also allowed. Corrugated metal may be used for secondary shed roofs and awnings.

\* New building materials may be allowed if compatible in appearance with surrounding buildings and if the material can be demonstrated to maintain its appearance over time.

\* Within the Historic District, choice of building materials is regulated by the Historic District Commission.

GUIDELINES FOR WICKFORD VILLAGE  
**Design Guidelines for Buildings**

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## "TRADEMARK" BUILDINGS

### *Guideline*

\* Trademark buildings are not allowed in Wickford Village. Buildings with forms that constitute an advertising icon are designed to look the same everywhere in the country and they detract from a cohesive and distinctive identity for the village. Rather, buildings housing national chain retailers must conform to these building and site design guidelines. Iconographic imagery should be limited to signage and should conform to the Signage guideline.

### *Discussion*

\* Small, separated and setback buildings surrounded by parking lots are incompatible with the village spatial character that these guidelines are intended to encourage.



*This building houses a McDonald's restaurant in Freeport, ME.*



*Buildings with forms that constitute an advertising icon are designed to look the same everywhere in the country.*

## **ENVIRONMENTAL DESIGN**

### *Guideline*

- \* Buildings should be designed to maximize natural daylighting and fresh air circulation. All buildings should have predominantly operable windows. Higher floor to ceiling dimensions increase daylight penetration and air circulation space. Buildings less than 60 ft. deep have better access to air, light and view and reduced heating, cooling and lighting requirements.
- \* Buildings should, where possible, be designed to take advantage of solar exposure, without sacrificing alignment to the street. South facing sloped roofs can be utilized for solar panels. Living areas which face south may use large windows for passive solar heat gain, but should be shaded in summer to prevent heat build-up.
- \* Material selections should evaluate embodied energy costs.
- \* Pressure treated wood should not be used. Alternatives include naturally resistant woods including cedar and redwood.

### *Discussion*

- \* By following simple environmental design guidelines, a building operator may considerably reduce costs and improve conditions for building users and occupants.

	Address	Plot; Lot	Building Space - All Commercial Structures (Square Footage - Assessor's Records)					Dwelling Units	Associated Parking (Visual Count)	
			Retail	Office	Whse	Rest. (Seating Capacity)	Bank			Service Garage
1.	35 Phillips Street	117; 173	0	0	0	0	0	0	Duplex	
2.	4 Brown Street	117; 177	1,712	182	384	0	0	0	One Apartment	5
3.	8 Brown Street	117; 178	160	0	0	0	0	0	Two-Family	6
4.	14 Brown Street	117; 179	0	0	0	0	0	0	Single -Family	
5.	22 Brown Street	117; 183	1,340	0	0	0	0	0	One Apartment	
6.	26 Brown Street	117; 287	952	0	278	0	0	0		
7.	30 Brown Street	117; 184	0	0	0	0	0	0	Duplex	
8.	38 Brown Street	117; 185	0	1,380	0	0	0	0	3 Apartments	
9.	50 Brown Street	117; 187	0	0	0	0	0	0	Single -Family	
10.	56 Brown Street	117; 188	0	1,536	0	0	0	0	One Apartment	4
11.	60 Brown Street	117; 189	0	1,406	0	0	0	0		
12.	64 - 66 Brown Street	117; 190	824	0	0	880 (20)	0	0		
13.	68 Brown Street	117; 285	0	1,440	0	1,257 (49)	0	0		
14.	0 Brown Street	117; 191	0	0	0	0	0	0	No Structures	
15.	70 Brown Street	117; 192	5,280	0	7,000	0	0	0	One Loading	2

	Address	Plat; Lot	Building Space - All Commercial Structures (Square Footage - Assessor's Records)					Dwelling Units	Associated Parking (Visual Count)	
			Retail	Office	Whse	Rest. (Seating Capacity)	Bank			Service Garage
16.	0 Brown Street	117; 193	0	0	0	0	0	0	No Structures	
17.	0 Brown Street	117; 194	0	0	0	0	0	0	No Structures	
18.	1 Brown Street	117; 199	4,565	0	360	0	0	1,092	6,700 SqFt- Multi-Unit	8
19.	17 Brown Street	117; 200	3,700	720	813	0	0	0		16
20.	23 Brown Street	117; 267	3,104	5,370	0	0	0	0		9
21.	27 Brown Street	117; 201	0	1,440	0	0	0	1,916		7
22.	0 Brown Street	117; 202	0	0	0	0	0	0	No Structures	6
23.	31 Brown Street	117; 203	0	170	0	0	0	0	1,422 SqFt - Boat Dock; Four Slips	
24.	35 Brown Street	117; 204	10,746	2,904	1,793	0	0	0		
25.	0 Brown Street	117; 205	0	0	0	0	0	0	No Structures	
26.	39 Brown Street	117; 206	0	0	0	0	0	0	No Structures??	24
27.	0 Brown Street	117; 207	0	0	0	0	0	0	500 SqFt - Boat Dock	
28.	0 Brown Street	117; 208	0	0	0	0	0	0	No Structures	85
29.	45 Brown Street	117; 209	1,640	1,500	0	0	0	0		

	Address	Plat; Lot	Building Space - All Commercial Structures (Square Footage - Assessor's Records)					Dwelling Units	Associated Parking (Visual Count)	
			Retail	Office	Whse	Rest. (Seating Capacity)	Bank			Service Garage
30.	0 Brown Street	117; 210	0	0	0	0	0	0	No Structures	
31.	55 Brown Street	117; 211	0	*5,240	0	0	0	0	Municipal	
32.	59 Brown Street	117; 212	7,802	0	0	0	0	0		20
33.	61 Brown Street	117; 288	0	1,260	0	0	0	0		6
34.	0 Brown Street	117; 213	0	0	0	0	0	0	No Structures	6
35.	0 Brown Street	117; 214	0	0	0	0	0	0	No Structures	35
36.	83 - 85 Brown Street	117; 215	3,000	0	1,060	800 (40)	0	0		1
37.	89 Brown Street	117; 216	2,020	0	1,332	0	0	0		1
38.	93 Brown Street	117; 217	2,030	0	0	0	0	0		
39.	95 Brown Street	117; 218	0	0	0	**750 (0)	0	0	***750 SqFt - Private Club	
40.	1 Main Street	117; 083	1,665	0	0	0	0	0	3,200 SqFt - Multi-Unit	
41.	7 Main Street	117; 219	2,448	1,748	792	0	0	0	1,748 SqFt - Multi-Unit	
42.	13 Main Street	117; 220	0	0	0	0	0	0	Multi - Family	
43.	6 Main Street	117; 158	915	0	0	0	0	0	2,000 SqFt -	

	Address	Plat; Lot	Building Space - All Commercial Structures (Square Footage - Assessor's Records)						Dwelling Units	Associated Parking (Visual Count)
			Retail	Office	Whse	Rest. (Seating Capacity)	Bank	Service Garage		
44.	2 Main Street	117; 157	512	0	56	0	0	0		
45.	1 West Main Street	117; 156	2,880	0	0	0	0	0	Two Apartments	
46.	13 West Main Street	117; 155	1,296	0	0	0	0	0	2,100 SqFt - 3 Apartments	
47.	15 West Main Street	117; 153	2,784	0	0	0	0	0	One Apartment	
48.	12 Main Street	117; 290	1,061	0	0	0	0	0		8
49.	10 Main Street	117; 291	1,171	0	0	0	0	0		9
50.	16 Main Street	117; 292	1,712	0	0	0	0	0		
51.	20 Main Street	117; 293	0	1,188	0	0	0	0		
52.	18 Main Street	117; 294	0	840	0	0	0	0		
53.	21 West Main Street	117; 152	2,314	0	870	1,456 (36)	0	0	One Apartment	19
54.	31 West Main Street	117; 151	1,591	1,300	0	0	0	0		12
55.	35 West Main Street	117; 150	0	0	3,416	0	0	0		
56.	41 West Main St.	117; 149	0	0	0	0	0	0	Storage Spaces:	15

	Address	Plat; Lot	Building Space - All Commercial Structures (Square Footage - Assessor's Records)						Dwelling Units	Associated Parking (Visual Count)	
			Retail	Office	W/Use	Rest. (Seating Capacity)	Bank	Service Garage			
57.	0 West Main Street	117; 148	862	0	0	0	0	0	1,045	Storage Spaces located on Lot 149	
58.	16 West Main Street	117; 195	2,290	0	0	0	924 (21)	0	0	Apartments	25
59.	22 West Main Street	117; 196	0	2,341	0	0	0	0	0		
60.	30 West Main Street	117, 197	1,430	0	760	0	0	0	0		
61.	0 West Main Street	117; 198	0	0	0	0	0	0	0	No Structures	
62.	0 Phillips Street	117; 029	0	0	0	0	0	0	0	No Structures	
63.	30 Phillips Street	117; 028	0	0	0	0	0	4,092	0		21
64.	14 Phillips Street	117; 027	648	0	261	0	0	0	0	1,750 SqFt - Multi-Unit	8
65.	10 Phillips Street	117; 227	970	0	396	0	0	0	0	2,040 SqFt - Multi-Unit	9
<b>Total:</b>			75,424	26,725	19,571	5,317 (166)	6,008	2,137	367		

Zoning Board of Review Parking Requirements	Existing Floor Space	Required Parking
RETAIL - One (1) Per Each 250 Square Feet:	75,424	302
OFFICE - One (1) Per Each 200 Square Feet:	26,725	134
WHSE - No Parking Required:	19,571	0
RESTAURANTS - One (1) Per Three (3) Seats Provided:	5,317	55
MUNICIPAL OFFICES - One (1) Per Each 150 Square Feet:	5,240	35
*COMMERCIAL DRIVE-IN (Majority of Service Over the Counter) - One (1) Per Each Ninety Square Feet or Minimum of Twenty (20) Spaces:	750	20
BANKS - One (1) Per Each 150 Square Feet:	6,008	40
GASOLINE FILLING & AUTOMOBILE REPAIR SHOPS - One (1) Storage Space Per Each 200 Square Feet:	2,137	11
**COMMERCIAL USE NOT EXPRESSLY PROVIDED FOR - One (1) Per Each 200 Square Feet:	750	4
	<b>Total:</b>	601
<b>Existing Off-Street Parking (-) Required Parking:</b>		-234
<b>ON-STREET PARKING</b>		
	A. Brown Street:	62
	B. Elam Street:	26
	C. Franklin Street:	9

<b>SATELLITE PARKING AREAS</b>			
<i>Area</i>	<i>Location</i>	<i>Distance in Miles - From Area to Front of Town Hall Annex</i>	<i>Approximate Number of Parking Spaces</i>
1. Cold Spring Community Center / Town Beach	30 Beach Street	.90	80
2. Electric Company Property - Private Property	Phillips Street - East of Wickford Elementary School	.15	15 (Partially Paved)
3. North Kingstown Free Library	100 Boone Street	.50	70
4. North Kingstown Town Hall	80 Boston Neck Road	.30	50
5. North Kingstown Town Wharf	Main Street - End of	.50	55
6. Wickford Elementary School	99 Phillips Street	.30	60
7. Wickford Middle School	250 Tower Hill Road	.75	150
8. Wilson Park	West Main Street and Post Road - Corner of	*.90 - from the furthest point in the park near the soccer field.  .55 - from the shortest point, West Main Street entrance.	Formal Section - 280  Waterfront Section - 80
		<b>Total:</b>	<b>840</b>

\* An approximately 250 foot long footpath exists connecting Wilson Park, near the soccer field, to Newtown Road. This footpath reduces the walking distance by approximately two-tenths of a mile.



Location : RTE. 1A/OLD TOWN <sup>HAEL</sup> <sup>Down St.</sup>  
 Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 JANUS File: 9990002A

BETA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900002  
 Start Date: 03/31/97  
 File I.D. :  
 Page : 1

Begin Time	03/31	Mon. Totl	04/01	Tues. Totl	04/02	Wed. Totl	04/03	Thur. Totl	04/04	Fri. Totl	Weekday Avg.	04/05	Sat. Totl	04/06	Sun. Totl
12:00 am		*		*		*		*		*		*	65		77
01:00		*		*		*		*		*		*	42		47
02:00		*		*		*		*		*		*	12		13
03:00		*		*		*		*		*		*	14		19
04:00		*		*		*		*		*		*	20		21
05:00		*		*		*		*		*		*	70		89
06:00		*		*		*		*		*		*	207		195
07:00		*		*		*		*		*		*	393		331
08:00		*		*		*		*		*		*	550		476
09:00		*		*		*		*		*		*	822		654
10:00		*		*		*		*		*		*	940		743
11:00		*		*		*		*		*		*	1037		776
12:00 pm		*		*		*		*		*		*	1050		789
01:00		*		*		*		*		*		*	1101		863
02:00		*		*		*		*		*		*	1139		782
03:00		*		*		*		*		*		*	1132		690
04:00		*		*		*		*		*		*	1011		535
05:00		*		*		*		*		1096	1096	*	772		426
06:00		*		*		*		*		881	881	*	619		299
07:00		*		*		*		*		524	524	*	395		237
08:00		*		*		*		*		359	359	*	285		119
09:00		*		*		*		*		259	259	*	213		79
10:00		*		*		*		*		182	182	*	184		55
11:00		*		*		*		*		146	146	*	113		29
Totals		0		0		0		0		3447	3447		12186		8344
Avg. WkDay		.0%		.0%		.0%		.0%		100.0%			353.5%		242.0%
AM Peaks													11:00		11:00
Volume													1037		776
PM Peaks										05:00	05:00		02:00		01:00
Volume										1096	1096		1139		863

Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 JANUS File: 9990002A

LINCOLN, RHODE ISLAND  
 WOOD, MASSACHUSETTS  
 #01-333-2382/617-255-1982

Site Code : 99900002  
 Start Date: 04/04/97  
 File I.D. :  
 Page : 2

Begin Time	Ch 1		Ch 2		Combined		Saturday
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	
12:00 04/05	22	280	0	0	22	280	
12:15	16	250	0	0	16	250	
12:30	18	254	0	0	18	254	
12:45	9 65	266 1050	0 *	0 *	9 65	266 1050	
01:00	15	268	0	0	15	268	
01:15	10	285	0	0	10	285	
01:30	7	276	0	0	7	276	
01:45	10 42	272 1101	0 *	0 *	10 42	272 1101	
02:00	4	284	0	0	4	284	
02:15	3	270	0	0	3	270	
02:30	1	293	0	0	1	293	
02:45	4 12	292 1139	0 *	0 *	4 12	292 1139	
03:00	6	277	0	0	6	277	
03:15	3	285	0	0	3	285	
03:30	4	310	0	0	4	310	
03:45	1 14	260 1132	0 *	0 *	1 14	260 1132	
04:00	1	265	0	0	1	265	
04:15	5	240	0	0	5	240	
04:30	5	272	0	0	5	272	
04:45	9 20	234 1011	0 *	0 *	9 20	234 1011	
05:00	4	199	0	0	4	199	
05:15	10	186	0	0	10	186	
05:30	32	217	0	0	32	217	
05:45	24 70	170 772	0 *	0 *	24 70	170 772	
06:00	35	192	0	0	35	192	
06:15	53	179	0	0	53	179	
06:30	42	124	0	0	42	124	
06:45	77 207	124 619	0 *	0 *	77 207	124 619	
07:00	86	120	0	0	86	120	
07:15	80	101	0	0	80	101	
07:30	115	90	0	0	115	90	
07:45	112 393	84 395	0 *	0 *	112 393	84 395	
08:00	94	74	0	0	94	74	
08:15	131	71	0	0	131	71	
08:30	138	75	0	0	138	75	
08:45	187 550	65 285	0 *	0 *	187 550	65 285	
09:00	177	60	0	0	177	60	
09:15	200	67	0	0	200	67	
09:30	205	39	0	0	205	39	
09:45	240 822	47 213	0 *	0 *	240 822	47 213	
10:00	254	45	0	0	254	45	
10:15	226	58	0	0	226	58	
10:30	208	48	0	0	208	48	
10:45	252 940	33 184	0 *	0 *	252 940	33 184	
11:00	258	35	0	0	258	35	
11:15	244	23	0	0	244	23	
11:30	268	29	0	0	268	29	
11:45	267 1037	26 113	0 *	0 *	267 1037	26 113	
Totals	4172	8014	0	0	4172	8014	
Day Totals		12186		0		12186	
Split %	100.0%	100.0%	.0%	.0%			

Peak Hour 11:00 02:45  
 Volume 1037 1164  
 P.H.F. .96 .93



Location : RIE. IN/OLD TOWN HALL  
 Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 JANUS File: 9990002A

DETA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900002  
 Start Date: 03/31/97  
 File I.D. :  
 Page : 2

Begin Time	04/07	Mon. Total	04/08	Tues. Total	04/09	Wed. Total	04/10	Thur. Total	04/11	Fri. Total	Weekday Avg.	04/12	Sat. Total	04/13	Sun. Total
12:00 am		12		14		17		21		*	16		*		*
01:00		12		5		2		2		*	5		*		*
02:00		16		9		11		11		*	12		*		*
03:00		13		14		10		12		*	12		*		*
04:00		68		77		72		69		*	72		*		*
05:00		310		302		305		310		*	307		*		*
06:00		671		700		717		681		*	692		*		*
07:00		705		731		722		701		*	715		*		*
08:00		695		664		716		*		*	692		*		*
09:00		711		736		725		*		*	724		*		*
10:00		769		759		739		*		*	756		*		*
11:00		854		813		840		*		*	836		*		*
12:00 pm		847		906		761		*		*	838		*		*
01:00		889		887		883		*		*	886		*		*
02:00		978		1014		910		*		*	967		*		*
03:00		989		938		954		*		*	960		*		*
04:00		911		876		914		*		*	900		*		*
05:00		696		763		715		*		*	725		*		*
06:00		481		450		535		*		*	489		*		*
07:00		311		362		297		*		*	323		*		*
08:00		257		289		251		*		*	266		*		*
09:00		119		138		118		*		*	125		*		*
10:00		63		46		71		*		*	60		*		*
11:00		47		27		31		*		*	35		*		*
Totals		11424		11520		11316		1807		0	11413		0		0
Avg. WkDay		100.1%		100.9%		99.1%		15.8%		.0%			.0%		.0%
AM Peaks		11:00		11:00		11:00		07:00			11:00				
Volume		854		813		840		701			836				
PM Peaks		03:00		02:00		03:00					02:00				
Volume		989		1014		954					967				

ADTs

Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 JANUS File: 9990002A

LINCOLN, KNODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Start Date: 04/04/97  
 File I.D. :  
 Page : 3

Begin Time	Ch 1				Ch 2				Combined				Sunday
	A.M.	P.H.	A.M.	P.H.	A.M.	P.H.	A.M.	P.H.	A.M.	P.H.	A.M.	P.H.	
12:00 04/06	26	206	0	0	26	206							
12:15	20	179	0	0	20	179							
12:30	16	218	0	0	16	218							
12:45	15	77 186	789	0	* 0	* 186	789	15	77	186	789		
01:00	13	195	0	0	13	195							
01:15	15	225	0	0	15	225							
01:30	17	224	0	0	17	224							
01:45	2	47 219	863	0	* 0	* 219	863	2	47	219	863		
02:00	1	192	0	0	1	192							
02:15	4	204	0	0	4	204							
02:30	3	200	0	0	3	200							
02:45	5	13 186	782	0	* 0	* 186	782	5	13	186	782		
03:00	4	181	0	0	4	181							
03:15	6	195	0	0	6	195							
03:30	7	161	0	0	7	161							
03:45	2	19 153	690	0	* 0	* 153	690	2	19	153	690		
04:00	1	174	0	0	1	174							
04:15	7	123	0	0	7	123							
04:30	5	112	0	0	5	112							
04:45	8	21 126	535	0	* 0	* 126	535	8	21	126	535		
05:00	8	114	0	0	8	114							
05:15	17	107	0	0	17	107							
05:30	25	118	0	0	25	118							
05:45	39	89 87	426	0	* 0	* 87	426	39	89	87	426		
06:00	38	86	0	0	38	86							
06:15	50	73	0	0	50	73							
06:30	52	75	0	0	52	75							
06:45	55	195 65	299	0	* 0	* 65	299	55	195	65	299		
07:00	68	65	0	0	68	65							
07:15	80	64	0	0	80	64							
07:30	101	62	0	0	101	62							
07:45	82	331 46	237	0	* 0	* 46	237	82	331	46	237		
08:00	82	34	0	0	82	34							
08:15	110	27	0	0	110	27							
08:30	138	22	0	0	138	22							
08:45	146	476 36	119	0	* 0	* 36	119	146	476	36	119		
09:00	164	22	0	0	164	22							
09:15	176	22	0	0	176	22							
09:30	158	20	0	0	158	20							
09:45	156	654 15	79	0	* 0	* 15	79	156	654	15	79		
10:00	158	25	0	0	158	25							
10:15	180	11	0	0	180	11							
10:30	191	11	0	0	191	11							
10:45	214	743 8	55	0	* 0	* 8	55	214	743	8	55		
11:00	196	10	0	0	196	10							
11:15	188	8	0	0	188	8							
11:30	221	8	0	0	221	8							
11:45	171	776 3	29	0	* 0	* 3	29	171	776	3	29		
Totals	3441	4903	0	0	3441	4903							
Day Totals		8344		0		8344							
Split %	100.0%	100.0%	.0%	.0%									

Peak Hour 10:45 01:00 10:45 01:00  
 Volume 819 863 819 863  
 P.H.F. .92 .95 .92 .95

Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 JANUS File: 9990002A

LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900002  
 Start Date: 04/04/97  
 File I.D. :  
 Page : 4

Begin Time	Ch 1		Ch 2		Combined		Monday
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	
12:00 04/07	3	198	0	0	3	198	
12:15	4	210	0	0	4	210	
12:30	4	196	0	0	4	196	
12:45	1	12 243	0	* 0	1	12 243	847
01:00	5	237	0	0	5	237	
01:15	4	210	0	0	4	210	
01:30	1	214	0	0	1	214	
01:45	2	12 228	0	* 0	2	12 228	889
02:00	2	255	0	0	2	255	
02:15	2	234	0	0	2	234	
02:30	8	234	0	0	8	234	
02:45	4	16 255	0	* 0	4	16 255	978
03:00	1	260	0	0	1	260	
03:15	0	246	0	0	0	246	
03:30	4	256	0	0	4	256	
03:45	8	13 227	0	* 0	8	13 227	989
04:00	6	262	0	0	6	262	
04:15	15	256	0	0	15	256	
04:30	22	204	0	0	22	204	
04:45	25	68 189	0	* 0	25	68 189	911
05:00	36	200	0	0	36	200	
05:15	79	172	0	0	79	172	
05:30	82	160	0	0	82	160	
05:45	113	310 164	0	* 0	113	310 164	696
06:00	160	140	0	0	160	140	
06:15	174	128	0	0	174	128	
06:30	161	112	0	0	161	112	
06:45	176	671 101	0	* 0	176	671 101	481
07:00	179	108	0	0	179	108	
07:15	180	74	0	0	180	74	
07:30	164	67	0	0	164	67	
07:45	182	705 62	0	* 0	182	705 62	311
08:00	184	82	0	0	184	82	
08:15	170	64	0	0	170	64	
08:30	170	56	0	0	170	56	
08:45	171	695 55	0	* 0	171	695 55	257
09:00	174	45	0	0	174	45	
09:15	192	33	0	0	192	33	
09:30	171	18	0	0	171	18	
09:45	174	711 23	0	* 0	174	711 23	119
10:00	176	26	0	0	176	26	
10:15	206	16	0	0	206	16	
10:30	202	9	0	0	202	9	
10:45	185	769 12	0	* 0	185	769 12	63
11:00	210	19	0	0	210	19	
11:15	202	9	0	0	202	9	
11:30	204	10	0	0	204	10	
11:45	238	854 9	0	* 0	238	854 9	47
Totals	4836	6588	0	0	4836	6588	
Day Totals	11424		0		11424		
Split %	100.0%	100.0%	.0%	.0%			

Peak Hour 11:00 02:45 11:00 02:45  
 Volume 854 1017 854 1017  
 P.H.F. .89 .97 .89 .97

Begin Time	Ch 1		Ch 2		Combined		Tuesday
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	
12:00 04/08	2	244	0	0	2	244	
12:15	4	204	0	0	4	204	
12:30	4	200	0	0	4	200	
12:45	4	258	0	*	4	258	906
01:00	3	222	0	0	3	222	
01:15	0	218	0	0	0	218	
01:30	0	183	0	0	0	183	
01:45	2	264	0	*	2	264	887
02:00	1	272	0	0	1	272	
02:15	1	236	0	0	1	236	
02:30	3	254	0	0	3	254	
02:45	4	252	0	*	4	252	1014
03:00	0	234	0	0	0	234	
03:15	0	213	0	0	0	213	
03:30	4	260	0	0	4	260	
03:45	10	231	0	*	10	231	938
04:00	11	193	0	0	11	193	
04:15	18	262	0	0	18	262	
04:30	23	243	0	0	23	243	
04:45	25	178	0	*	25	178	876
05:00	46	190	0	0	46	190	
05:15	69	172	0	0	69	172	
05:30	75	200	0	0	75	200	
05:45	112	201	0	*	112	201	763
06:00	172	136	0	0	172	136	
06:15	191	125	0	0	191	125	
06:30	169	93	0	0	169	93	
06:45	168	96	0	*	168	96	450
07:00	182	94	0	0	182	94	
07:15	172	105	0	0	172	105	
07:30	174	85	0	0	174	85	
07:45	203	78	0	*	203	78	362
08:00	186	126	0	0	186	126	
08:15	153	71	0	0	153	71	
08:30	165	50	0	0	165	50	
08:45	160	42	0	*	160	42	289
09:00	188	40	0	0	188	40	
09:15	184	37	0	0	184	37	
09:30	188	35	0	0	188	35	
09:45	176	26	0	*	176	26	138
10:00	177	13	0	0	177	13	
10:15	182	15	0	0	182	15	
10:30	206	8	0	0	206	8	
10:45	194	10	0	*	194	10	46
11:00	207	13	0	0	207	13	
11:15	199	5	0	0	199	5	
11:30	194	6	0	0	194	6	
11:45	213	3	0	*	213	3	27
Totals	4824	6696	0	0	4824	6696	
Day Totals		11520		0		11520	
Split %	100.0%	100.0%	.0%	.0%			

Peak Hour	11:00	01:45	11:00	01:45
Volume	813	1026	813	1026
P.H.F.	.95	.94	.95	.94

Location : ...  
 Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 ANUS File: 9990002A

LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900002  
 Start Date: 04/04/97  
 File I.D. :  
 Page : 6

Time	Ch 1		Ch 2		Combined		Wednesday					
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.						
2:00 04/09	4	196	0	0	4	196						
2:15	6	180	0	0	6	180						
2:30	5	158	0	0	5	158						
2:45	2	17	227	761	0	*	0	*	2	17	227	761
1:00	2	224	0	0	2	224						
1:15	0	196	0	0	0	196						
1:30	0	224	0	0	0	224						
1:45	0	2	239	883	0	*	0	*	0	2	239	883
2:00	4	225	0	0	4	225						
2:15	3	234	0	0	3	234						
2:30	2	228	0	0	2	228						
2:45	2	11	223	910	0	*	0	*	2	11	223	910
3:00	1	220	0	0	1	220						
3:15	0	238	0	0	0	238						
3:30	5	276	0	0	5	276						
3:45	4	10	220	954	0	*	0	*	4	10	220	954
4:00	3	236	0	0	3	236						
4:15	18	251	0	0	18	251						
4:30	20	210	0	0	20	210						
4:45	31	72	217	914	0	*	0	*	31	72	217	914
5:00	44	216	0	0	44	216						
5:15	70	177	0	0	70	177						
5:30	84	164	0	0	84	164						
5:45	107	305	158	715	0	*	0	*	107	305	158	715
6:00	148	155	0	0	148	155						
6:15	208	150	0	0	208	150						
6:30	185	134	0	0	185	134						
6:45	176	717	96	535	0	*	0	*	176	717	96	535
7:00	170	82	0	0	170	82						
7:15	179	74	0	0	179	74						
7:30	171	73	0	0	171	73						
7:45	202	722	68	297	0	*	0	*	202	722	68	297
8:00	182	80	0	0	182	80						
8:15	176	67	0	0	176	67						
8:30	184	54	0	0	184	54						
8:45	174	716	50	251	0	*	0	*	174	716	50	251
9:00	178	32	0	0	178	32						
9:15	183	33	0	0	183	33						
9:30	194	34	0	0	194	34						
9:45	170	725	19	118	0	*	0	*	170	725	19	118
10:00	188	26	0	0	188	26						
10:15	196	19	0	0	196	19						
10:30	186	12	0	0	186	12						
10:45	169	739	14	71	0	*	0	*	169	739	14	71
11:00	192	13	0	0	192	13						
11:15	200	4	0	0	200	4						
11:30	220	8	0	0	220	8						
11:45	228	840	6	31	0	*	0	*	228	840	6	31
Totals	4876	6440	0	0	4876	6440						
Day Totals	11316		0		11316							
Split %	100.0%	100.0%	.0%	.0%								

Peak Hour 11:00 03:30 11:00 03:30  
 Volume 840 983 840 983  
 P.H.F. .92 .89 .92 .89

Weather : SUNNY AND CLEAR  
 Counter : DELTA  
 JANUS File: 9990002A

LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900002  
 Start Date: 04/04/97  
 File I.D. :  
 Page : 7

Begin Time	Ch 1		Ch 2		Combined		Thursday
	A.M.	P.H.	A.M.	P.M.	A.M.	P.M.	
12:00 04/10	8	*	0	*	8	*	
12:15	5	*	0	*	5	*	
12:30	5	*	0	*	5	*	
12:45	3	21	0	*	3	21	*
01:00	1	*	0	*	1	*	
01:15	0	*	0	*	0	*	
01:30	0	*	0	*	0	*	
01:45	1	2	0	*	1	2	*
02:00	2	*	0	*	2	*	
02:15	5	*	0	*	5	*	
02:30	1	*	0	*	1	*	
02:45	3	11	0	*	3	11	*
03:00	2	*	0	*	2	*	
03:15	2	*	0	*	2	*	
03:30	5	*	0	*	5	*	
03:45	3	12	0	*	3	12	*
04:00	10	*	0	*	10	*	
04:15	14	*	0	*	14	*	
04:30	16	*	0	*	16	*	
04:45	29	69	0	*	29	69	*
05:00	51	*	0	*	51	*	
05:15	60	*	0	*	60	*	
05:30	88	*	0	*	88	*	
05:45	111	310	0	*	111	310	*
06:00	151	*	0	*	151	*	
06:15	194	*	0	*	194	*	
06:30	157	*	0	*	157	*	
06:45	179	681	0	*	179	681	*
07:00	164	*	0	*	164	*	
07:15	175	*	0	*	175	*	
07:30	159	*	0	*	159	*	
07:45	203	701	0	*	203	701	*
08:00	195	*	0	*	195	*	
08:15	*	*	*	*	*	*	
08:30	*	*	*	*	*	*	
08:45	*	*	*	*	*	195	*
09:00	*	*	*	*	*	*	
09:15	*	*	*	*	*	*	
09:30	*	*	*	*	*	*	
09:45	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	
10:15	*	*	*	*	*	*	
10:30	*	*	*	*	*	*	
10:45	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	
11:15	*	*	*	*	*	*	
11:30	*	*	*	*	*	*	
11:45	*	*	*	*	*	*	*
Totals	2002	0	0	0	2002	0	
Day Totals	2002		0		2002		
Split %	100.0%	*	.0%	*			

Peak Hour 07:15  
 Volume 732  
 P.H.F. .90

07:15  
 732  
 .90

(HUSSEY BRIDGE)

Begin Time	Mon. 03/31		Tues.		Wed.		Thur.		Fri.		Sat.		Sun.		Avg.		
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	
12:00	*	*	*	*	*	*	*	*	*	*	34	266	35	200	34	233	
12:15	*	*	*	*	*	*	*	*	*	*	33	260	34	234	34	247	
12:30	*	*	*	*	*	*	*	*	*	*	19	302	24	227	22	264	
12:45	*	*	*	*	*	*	*	*	*	*	15	300	20	212	18	256	
01:00	*	*	*	*	*	*	*	*	*	*	19	296	15	220	17	258	
01:15	*	*	*	*	*	*	*	*	*	*	18	286	27	228	22	257	
01:30	*	*	*	*	*	*	*	*	*	*	19	284	22	218	20	251	
01:45	*	*	*	*	*	*	*	*	*	*	15	280	9	228	12	254	
02:00	*	*	*	*	*	*	*	*	*	*	8	292	5	212	6	252	
02:15	*	*	*	*	*	*	*	*	*	*	5	298	8	201	6	250	
02:30	*	*	*	*	*	*	*	*	*	*	7	282	8	222	8	252	
02:45	*	*	*	*	*	*	*	*	*	*	6	334	10	200	8	267	
03:00	*	*	*	*	*	*	*	*	*	*	8	317	5	192	6	254	
03:15	*	*	*	*	*	*	*	*	*	*	5	313	9	217	7	265	
03:30	*	*	*	*	*	*	*	*	*	*	2	299	10	184	6	242	
03:45	*	*	*	*	*	*	*	*	*	*	2	284	3	200	2	242	
04:00	*	*	*	*	*	*	*	*	*	*	37	1	305	1	202	1	181
04:15	*	*	*	*	*	*	*	*	*	*	317	6	259	8	159	7	245
04:30	*	*	*	*	*	*	*	*	*	*	318	11	278	7	135	9	244
04:45	*	*	*	*	*	*	*	*	*	*	333	12	260	8	136	10	243
05:00	*	*	*	*	*	*	*	*	*	*	288	7	266	11	126	9	227
05:15	*	*	*	*	*	*	*	*	*	*	302	16	219	17	126	16	216
05:30	*	*	*	*	*	*	*	*	*	*	294	31	238	24	126	28	219
05:45	*	*	*	*	*	*	*	*	*	*	288	23	219	27	133	25	213
06:00	*	*	*	*	*	*	*	*	*	*	310	39	204	38	120	38	211
06:15	*	*	*	*	*	*	*	*	*	*	295	53	198	40	105	46	199
06:30	*	*	*	*	*	*	*	*	*	*	256	46	176	54	80	50	171
06:45	*	*	*	*	*	*	*	*	*	*	230	81	160	58	72	70	154
07:00	*	*	*	*	*	*	*	*	*	*	220	88	150	62	80	75	150
07:15	*	*	*	*	*	*	*	*	*	*	151	103	118	76	76	90	115
07:30	*	*	*	*	*	*	*	*	*	*	146	116	121	84	78	100	115
07:45	*	*	*	*	*	*	*	*	*	*	146	134	114	95	56	114	105
08:00	*	*	*	*	*	*	*	*	*	*	150	122	93	98	50	110	98
08:15	*	*	*	*	*	*	*	*	*	*	118	146	82	114	52	130	84
08:30	*	*	*	*	*	*	*	*	*	*	92	162	80	135	44	148	72
08:45	*	*	*	*	*	*	*	*	*	*	122	214	83	149	50	182	85
09:00	*	*	*	*	*	*	*	*	*	*	124	194	70	160	35	177	76
09:15	*	*	*	*	*	*	*	*	*	*	72	232	95	150	27	191	65
09:30	*	*	*	*	*	*	*	*	*	*	74	224	55	152	30	188	53
09:45	*	*	*	*	*	*	*	*	*	*	69	244	66	182	21	213	52
10:00	*	*	*	*	*	*	*	*	*	*	54	285	63	166	24	226	47
10:15	*	*	*	*	*	*	*	*	*	*	72	238	67	210	13	224	51
10:30	*	*	*	*	*	*	*	*	*	*	62	245	72	200	16	222	50
10:45	*	*	*	*	*	*	*	*	*	*	57	262	44	212	14	237	38
11:00	*	*	*	*	*	*	*	*	*	*	55	270	45	206	9	238	36
11:15	*	*	*	*	*	*	*	*	*	*	56	234	40	216	11	225	36
11:30	*	*	*	*	*	*	*	*	*	*	56	276	39	216	9	246	35
11:45	*	*	*	*	*	*	*	*	*	*	39	289	34	204	7	246	27

Totals	0	0	0	0	0	0	0	0	0	0	5203	4619	9006	3624	5617	4119	7957	
Day Totals	0		0		0		0		0		5203	13625		9241		12076		
Percent Spl	*	*	*	*	*	*	*	*	*	*	.0%	100.0%	33.9%	66.1%	39.2%	60.7%	34.1%	65.8%

Peaks											04:15	11:00	02:45	10:45	01:00	11:00
Volume											1256	1069	1263	850	894	955
P.H.F.											.94	.92	.94	.98	.98	.97

Location : RTE. 1A E. OF GREEN BRIDGE  
 Weather : SUNNY AND CLEAR  
 Counter : BETA  
 JANUS File: 9990001A

DATA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900001  
 Start Date: 03/31/97  
 File I.D. : 9990001C  
 Page : 2

Begin Time	Mon. 04/07		Tues.		Wed.		Thur.		Fri.		Sat.		Sun.		Avg.	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	5	228	4	256	6	219	9	*	*	*	*	*	*	*	6	234
12:15	8	224	13	203	11	178	11	*	*	*	*	*	*	*	11	202
12:30	7	196	6	204	6	163	2	*	*	*	*	*	*	*	5	188
12:45	4	298	2	266	0	274	2	*	*	*	*	*	*	*	2	279
01:00	3	252	5	268	1	250	3	*	*	*	*	*	*	*	3	257
01:15	5	256	4	249	1	218	3	*	*	*	*	*	*	*	3	241
01:30	3	273	1	239	0	262	1	*	*	*	*	*	*	*	1	258
01:45	1	287	0	278	1	246	2	*	*	*	*	*	*	*	1	270
02:00	3	273	4	284	3	275	3	*	*	*	*	*	*	*	3	277
02:15	4	257	1	264	3	262	3	*	*	*	*	*	*	*	3	261
02:30	6	290	5	294	2	304	2	*	*	*	*	*	*	*	4	296
02:45	3	265	4	274	3	278	5	*	*	*	*	*	*	*	4	272
03:00	1	296	4	251	0	254	2	*	*	*	*	*	*	*	2	267
03:15	2	290	1	262	4	234	2	*	*	*	*	*	*	*	2	262
03:30	10	288	5	293	7	304	5	*	*	*	*	*	*	*	7	295
03:45	10	279	10	260	7	265	4	*	*	*	*	*	*	*	8	268
04:00	9	314	13	235	8	286	15	*	*	*	*	*	*	*	11	278
04:15	22	300	24	284	17	314	16	*	*	*	*	*	*	*	20	299
04:30	22	245	35	260	26	224	30	*	*	*	*	*	*	*	28	243
04:45	35	236	31	245	40	248	40	*	*	*	*	*	*	*	36	243
05:00	43	232	50	231	48	256	57	*	*	*	*	*	*	*	50	240
05:15	92	236	89	200	94	230	73	*	*	*	*	*	*	*	87	222
05:30	104	222	109	218	122	206	121	*	*	*	*	*	*	*	114	215
05:45	146	230	153	246	144	202	160	*	*	*	*	*	*	*	151	226
06:00	216	178	230	196	221	188	216	*	*	*	*	*	*	*	221	187
06:15	250	177	294	156	284	186	278	*	*	*	*	*	*	*	276	173
06:30	220	166	224	130	258	144	221	*	*	*	*	*	*	*	231	147
06:45	242	168	242	115	262	124	253	*	*	*	*	*	*	*	250	136
07:00	254	135	226	145	252	104	238	*	*	*	*	*	*	*	242	128
07:15	208	98	221	138	207	112	230	*	*	*	*	*	*	*	216	116
07:30	216	111	218	112	217	99	197	*	*	*	*	*	*	*	212	107
07:45	209	80	224	110	236	83	230	*	*	*	*	*	*	*	225	91
08:00	196	107	192	162	202	114	0	*	*	*	*	*	*	*	148	128
08:15	184	92	188	93	184	106	*	*	*	*	*	*	*	*	185	97
08:30	174	79	186	65	198	72	*	*	*	*	*	*	*	*	186	72
08:45	180	72	178	56	190	74	*	*	*	*	*	*	*	*	183	67
09:00	198	56	212	61	175	48	*	*	*	*	*	*	*	*	195	55
09:15	174	45	198	42	194	53	*	*	*	*	*	*	*	*	189	47
09:30	183	33	190	57	184	49	*	*	*	*	*	*	*	*	186	46
09:45	174	32	172	38	175	32	*	*	*	*	*	*	*	*	174	34
10:00	188	34	198	26	182	35	*	*	*	*	*	*	*	*	189	32
10:15	212	29	165	26	186	33	*	*	*	*	*	*	*	*	188	29
10:30	190	19	198	11	200	18	*	*	*	*	*	*	*	*	196	16
10:45	189	17	178	23	190	17	*	*	*	*	*	*	*	*	186	19
11:00	206	24	222	16	200	20	*	*	*	*	*	*	*	*	209	20
11:15	206	13	202	8	184	14	*	*	*	*	*	*	*	*	197	12
11:30	220	13	208	9	202	11	*	*	*	*	*	*	*	*	210	11
11:45	250	12	218	8	224	7	*	*	*	*	*	*	*	*	231	9
Totals	5487	8057	5557	7867	5561	7695	2434	0	0	0	0	0	0	0	5487	7872
Day Totals	13544		13424		13256		2434	0	0	0	0	0	0	0	13359	
Percent Spl	40.5%	59.4%	41.4%	58.6%	41.9%	58.0%	100.0%	.0%	*	*	*	*	*	*	41.0%	58.9%
Peaks	06:15	03:30	06:00	01:45	06:15	03:30	06:15									06:15
Volume	966	1181	990	1120	1056	1169	990									999
P.H.F.	.95	.94	.84	.95	.92	.93	.89									.90
ADTs																

Weather : SUNNY AND CLEAR  
 Counter : BETA  
 JANUS File: 9990001A

DATA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900001  
 Start Date: 04/04/97  
 File I.D. : 9990001C  
 Page : 1

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th		
12:00 04/04	*	*	*	*	*	
01:00	*	*	*	*	*	
02:00	*	*	*	*	*	
03:00	*	*	*	*	*	
04:00	*	*	*	*	*	
05:00	*	*	*	*	*	
06:00	*	*	*	*	*	
07:00	*	*	*	*	*	
08:00	*	*	*	*	*	
09:00	*	*	*	*	*	
10:00	*	*	*	*	*	
11:00	*	*	*	*	*	
12:00	*	*	*	*	*	
01:00	*	*	*	*	*	
02:00	*	*	*	*	*	
03:00	*	*	*	*	*	
04:00	37	317	318	333	1005	*****
05:00	288	302	294	288	1172	*****
06:00	310	295	256	230	1091	*****
07:00	220	151	146	146	663	*****
08:00	150	118	92	122	482	*****
09:00	124	72	74	69	339	*****
10:00	54	72	62	57	245	*****
11:00	55	56	56	39	206	*****
Total					5203	

AM Peak There was not enough data to calculate the AM peak hour.

PM Peak The PM peak hour began 04:15.  
 The peak volume was 1256.  
 The largest interval began 04:45,  
 and contained 333 vehicles.  
 The peak hour factor was .94

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th	Total	
12:00 04/05	34	33	19	15	101	****
01:00	19	18	19	15	71	***
02:00	8	5	7	6	26	*
03:00	8	5	2	2	17	*
04:00	1	6	11	12	30	*
05:00	7	16	31	23	77	***
06:00	39	53	46	81	219	*****
07:00	88	103	116	134	441	*****
08:00	122	146	162	214	644	*****
09:00	194	232	224	244	894	*****
10:00	285	238	245	262	1030	*****
11:00	270	234	276	289	1069	*****
12:00	266	260	302	300	1128	*****
01:00	296	286	284	280	1146	*****
02:00	292	298	282	334	1206	*****
03:00	317	313	299	284	1213	*****
04:00	305	259	278	260	1102	*****
05:00	266	219	238	219	942	*****
06:00	204	198	176	160	738	*****
07:00	150	118	121	114	503	*****
08:00	93	82	80	83	338	*****
09:00	70	95	55	66	286	*****
10:00	63	67	72	44	246	*****
11:00	45	40	39	34	158	*****
Total					13625	

AM Peak The AM peak hour began 11:45.  
 The peak volume was 1117.  
 The largest interval began 12:30,  
 and contained 302 vehicles.  
 The peak hour factor was .92

PM Peak The PM peak hour began 02:45.  
 The peak volume was 1263.  
 The largest interval began 02:45,  
 and contained 334 vehicles.  
 The peak hour factor was .95

LOCATION : Rte. 1A E. of GREEN DRIVE  
 Weather : SUNNY AND CLEAR  
 Counter : BETA  
 JANUS File: 9990001A

DETA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900001  
 Start Date: 04/04/97  
 File I.D. : 9990001C  
 Page : 3

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th		
12:00 04/06	35	34	24	20	113	****
01:00	15	27	22	9	73	***
02:00	5	8	8	10	31	*
03:00	5	9	10	3	27	*
04:00	1	8	7	8	24	*
05:00	11	17	24	27	79	***
06:00	38	40	54	58	190	*****
07:00	62	76	84	95	317	*****
08:00	98	114	135	149	496	*****
09:00	160	150	152	182	644	*****
10:00	166	210	200	212	788	*****
11:00	206	216	216	204	842	*****
12:00	200	234	227	212	873	*****
01:00	220	228	218	228	894	*****
02:00	212	201	222	200	835	*****
03:00	192	217	184	200	793	*****
04:00	202	159	135	136	632	*****
05:00	126	126	126	133	511	*****
06:00	120	105	80	72	377	*****
07:00	80	76	78	56	290	*****
08:00	50	52	44	50	196	*****
09:00	35	27	30	21	113	*****
10:00	24	13	16	14	67	***
11:00	9	11	9	7	36	*
Total					9241	

AM Peak The AM peak hour began 11:45.  
 The peak volume was 865.  
 The largest interval began 12:15,  
 and contained 234 vehicles.  
 The peak hour factor was .92

PM Peak The PM peak hour began 01:00.  
 The peak volume was 894.  
 The largest interval began 01:15,  
 and contained 228 vehicles.  
 The peak hour factor was .98

Location : RTE. 1A E. OF GREEN BRIDE  
 Weather : SUNNY AND CLEAR  
 Counter : BETA  
 JANUS File: 9990001A

BETA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900001  
 Start Date: 04/04/97  
 File I.D. : 9990001C  
 Page : 4

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th		
12:00 04/07	5	8	7	4	24	*
01:00	3	5	3	1	12	
02:00	3	4	6	3	16	*
03:00	1	2	10	10	23	*
04:00	9	22	22	35	88	****
05:00	43	92	104	146	385	*****
06:00	216	250	220	242	928	*****
07:00	254	208	216	209	887	*****
08:00	196	184	174	180	734	*****
09:00	198	174	183	174	729	*****
10:00	188	212	190	189	779	*****
11:00	206	206	220	250	882	*****
12:00	228	224	196	298	946	*****
01:00	252	256	273	287	1068	*****
02:00	273	257	290	265	1085	*****
03:00	296	290	288	279	1153	*****
04:00	314	300	245	236	1095	*****
05:00	232	236	222	230	920	*****
06:00	178	177	166	168	689	*****
07:00	135	98	111	80	424	*****
08:00	107	92	79	72	350	*****
09:00	56	45	33	32	166	*****
10:00	34	29	19	17	99	****
11:00	24	13	13	12	62	**
Total					13544	

AM Peak The AM peak hour began 06:15.  
 The peak volume was 966.  
 The largest interval began 07:00,  
 and contained 254 vehicles.  
 The peak hour factor was .95

PM Peak The PM peak hour began 03:30.  
 The peak volume was 1181.  
 The largest interval began 04:00,  
 and contained 314 vehicles.  
 The peak hour factor was .94

Location : RTE. 1A E. OF GREEN BRIDE  
 Weather : SUNNY AND CLEAR  
 Counter : BETA  
 JANUS File: 9990001A

BETA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900001  
 Start Date: 04/04/97  
 File I.D. : 9990001C  
 Page : 5

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th		
12:00 04/08	4	13	6	2	25	*
01:00	5	4	1	0	10	
02:00	4	1	5	4	14	*
03:00	4	1	5	10	20	*
04:00	13	24	35	31	103	****
05:00	50	89	109	153	401	*****
06:00	230	294	224	242	990	*****
07:00	226	221	218	224	889	*****
08:00	192	188	186	178	744	*****
09:00	212	198	190	172	772	*****
10:00	198	165	198	178	739	*****
11:00	222	202	208	218	850	*****
12:00	256	203	204	266	929	*****
01:00	268	249	239	278	1034	*****
02:00	284	264	294	274	1116	*****
03:00	251	262	293	260	1066	*****
04:00	235	284	260	245	1024	*****
05:00	231	200	218	246	895	*****
06:00	196	156	130	115	597	*****
07:00	145	138	112	110	505	*****
08:00	162	93	65	56	376	*****
09:00	61	42	57	38	198	*****
10:00	26	26	11	23	86	***
11:00	16	8	9	8	41	**
Total					13424	

AM Peak The AM peak hour began 06:00.  
 The peak volume was 990.  
 The largest interval began 06:15,  
 and contained 294 vehicles.  
 The peak hour factor was .84

PM Peak The PM peak hour began 01:45.  
 The peak volume was 1120.  
 The largest interval began 02:30,  
 and contained 294 vehicles.  
 The peak hour factor was .95

Location : RTE. 1A E. OF GREEN BRIDE  
 Weather : SUNNY AND CLEAR  
 Counter : BETA  
 JANUS File: 9990001A

BETA ENGINEERING, INC.  
 LINCOLN, RHODE ISLAND  
 NORWOOD, MASSACHUSETTS  
 401-333-2382/617-255-1982

Site Code : 99900001  
 Start Date: 04/04/97  
 File I.D. : 9990001C  
 Page : 6

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th		
12:00 04/09	6	11	6	0	23	*
01:00	1	1	0	1	3	
02:00	3	3	2	3	11	
03:00	0	4	7	7	18	*
04:00	8	17	26	40	91	****
05:00	48	94	122	144	408	*****
06:00	221	284	258	262	1025	*****
07:00	252	207	217	236	912	*****
08:00	202	184	198	190	774	*****
09:00	175	194	184	175	728	*****
10:00	182	186	200	190	758	*****
11:00	200	184	202	224	810	*****
12:00	219	178	163	274	834	*****
01:00	250	218	262	246	976	*****
02:00	275	262	304	278	1119	*****
03:00	254	234	304	265	1057	*****
04:00	286	314	224	248	1072	*****
05:00	256	230	206	202	894	*****
06:00	188	186	144	124	642	*****
07:00	104	112	99	83	398	*****
08:00	114	106	72	74	366	*****
09:00	48	53	49	32	182	*****
10:00	35	33	18	17	103	****
11:00	20	14	11	7	52	**
Total					13256	

AM Peak The AM peak hour began 06:15.  
 The peak volume was 1056.  
 The largest interval began 06:15,  
 and contained 284 vehicles.  
 The peak hour factor was .93

PM Peak The PM peak hour began 03:30.  
 The peak volume was 1169.  
 The largest interval began 04:15,  
 and contained 314 vehicles.  
 The peak hour factor was .93

Begin Time	<-----Quarter Hour ----->				Hour Total	Each * Equals 25 Vehicles
	1st	2nd	3rd	4th		
12:00 04/10	9	11	2	2	24	*
01:00	3	3	1	2	9	
02:00	3	3	2	5	13	*
03:00	2	2	5	4	13	*
04:00	15	16	30	40	101	****
05:00	57	73	121	160	411	*****
06:00	216	278	221	253	968	*****
07:00	238	230	197	230	895	*****
08:00	0	*	*	*	*	
09:00	*	*	*	*	*	
10:00	*	*	*	*	*	
11:00	*	*	*	*	*	
12:00	*	*	*	*	*	
01:00	*	*	*	*	*	
02:00	*	*	*	*	*	
03:00	*	*	*	*	*	
04:00	*	*	*	*	*	
05:00	*	*	*	*	*	
06:00	*	*	*	*	*	
07:00	*	*	*	*	*	
08:00	*	*	*	*	*	
09:00	*	*	*	*	*	
10:00	*	*	*	*	*	
11:00	*	*	*	*	*	
Total					2434	

AM Peak The AM peak hour began 06:15.  
 The peak volume was 990.  
 The largest interval began 06:15,  
 and contained 278 vehicles.  
 The peak hour factor was .89

PM Peak There was not enough data to calculate the PM peak hour.



CHIEF OF POLICE  
James L. Wynne

## NORTH KINGSTOWN POLICE DEPARTMENT

8166 POST ROAD, NORTH KINGSTOWN, RHODE ISLAND 02852  
Telephone: (401) 294-3311 FAX: (401) 294-6830

Date December 19, 1997

To: Chief Wynne

From: Sgt Brennan

Subject: Brown St.

Chief:

The measurements for the areas you requested on Brown St are listed below.

### CROSSWALKS

Brown St. from bookstore to Updike Park	65'10"
Brown St. at Ryan's Market	45'
Brown St. at Wilson's of Wickford	34'
Brown St from Gold Lady to Phillips St.	51'

\*all crosswalks approximately 7'8" wide

### PARKING SPOTS

Parking spots on Brown St. are approximately 8'4" wide

Respectfully Submitted,

*Sgt Charles Brennan*  
Sgt. Charles Brennan

**BROWN STREET ACCIDENTS**  
MAY 14, 1991 TO JANUARY 28, 1997

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestrians
19970118	1400	7	02	00	00
19970113	1202	2	02	00	00
19961209	1030	2	02	00	00
19961204	1348	4	02	00	00
19961201	1600	2	02	00	00
19961129	1215	6	02	00	00
19961112	1712	3	02	00	00
19961108	1505	6	02	01	00
19961027	0829	1	02	00	00
19961015	1150	3	02	00	00
19961011	0855	6	02	00	00
19960924	1516	3	02	00	00
19960914	2313	7	02	00	00
19960831	1130	7	02	00	00
19960818	1526	1	02	00	00
19960808	1708	5	02	00	00
19960722	1605	2	02	00	00
19960622	1400	7	02	00	00
19960614	1200	6	02	00	00
19960607	0924	6	02	00	00
19960602	1445	1	02	00	00
19960529	1106	4	02	00	00
19960518	1235	7	02	00	00
19960503	1117	6	02	00	00
19960429	1642	2	02	00	00
19960424	1514	4	02	00	00
19960419	1258	6	02	00	00
19960408	1020	2	02	00	00
19960202	1200	6	02	00	00
19960117	1630	4	02	00	00
19951229	1224	6	02	00	00
19951219	1959	3	02	00	00
19951218	1315	2	02	00	00
19951218	1255	2	02	00	00
19951212	1742	3	02	01	00
19951206	1207	4	02	00	00
19951124	1300	6	01	00	00
19951112	1217	1	02	00	00
19951028	1145	7	02	00	00
19951023	0915	2	02	00	00
19951010	1200	3	02	00	00
19951005	1549	5	02	00	00
19950818	1215	2	02	00	00
19950807	1200	2	02	00	00
19950716	1949	1	02	02	00
19950625	1615	1	02	01	00
19950523	1718	3	01	01	01

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestians
19950117	1104	3	02	00	00
19941228	1610	4	02	00	00
19941205	0922	2	02	00	00
19941122	1715	3	02	00	00
19941027	0838	5	02	00	00
19941024	0720	2	01	00	00
19941014	1651	6	02	00	00
19941008	0928	7	02	00	00
19940929	1130	5	02	00	00
19940810	1552	4	02	00	00
19940730	1235	7	02	00	00
19940720	1014	4	02	00	00
19940614	0930	3	01	00	00
19940607	2025	3	02	00	00
19940521	1509	7	02	00	00
19940518	2338	4	02	01	00
19940516	1343	2	02	00	00
19940415	1700	6	02	00	00
19940327	1234	1	02	00	00
19940213	0030	1	02	00	00
19940213	0030	1	02	00	00
19940118	1608	3	02	00	00
19931217	1419	6	01	01	01
19931002	1700	7	02	00	00
19930827	2124	6	02	00	00
19930814	1822	7	02	00	00
19930714	1800	4	02	00	00
19930622	1652	3	02	00	00
19930620	1245	1	02	00	00
19930614	1045	2	02	00	00
19930608	1600	4	02	00	00
19930607	1404	2	02	00	00
19930605	1528	7	02	00	00
19930507	1638	6	02	00	00
19930422	0850	5	02	00	00
19921204	0700	6	02	00	00
19921119	1623	5	03	00	00
19921113	1605	6	02	00	00
19921113	1607	6	02	00	00
19921102	1030	2	02	00	00
19921030	1830	6	02	01	00
19921026	1300	2	02	00	00
19921015	1300	5	01	00	00
19921014	1022	4	02	00	00
19920903	1315	5	02	00	00
19920814	1020	6	02	00	00
19920731	1400	6	02	00	00
19920713	1205	2	02	02	00
19920703	1115	6	02	00	00

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestians
19920702	1910	5	02	00	00
19920624	1250	4	02	00	00
19920411	1610	7	02	00	00
19920109	1250	5	02	00	00
19920107	1130	3	02	01	00
19911228	1620	7	02	00	00
19911218	1500	4	02	00	00
19911121	1205	5	02	00	00
19911117	1030	1	02	00	00
19911115	1500	6	02	00	00
19911011	2332	6	01	02	00
19910920	1048	6	03	00	00
19910828	1555	4	02	00	00
19910820	0722	3	02	00	00
19910718	1317	5	02	00	00
19910712	1521	6	1	1	1
19910607	1632	6	02	00	00
19910514	1102	3	2	0	0

Count: 114

**WEST MAIN STREET ACCIDENTS**  
MAY 1991 TO JANUARY 28, 1997

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestians
19961227	1110	6	02	00	00
19961218	1149	4	02	01	00
19961202	1333	2	02	00	00
19961126	1358	3	02	00	00
19960919	1353	5	01	00	00
19960913	1600	6	02	00	00
19960907	1248	7	02	00	00
19960719	1419	6	02	00	00
19960625	1535	3	02	01	00
19960501	0816	4	02	01	00
19960422	1101	2	02	00	00
19960214	1245	4	02	00	00
19960110	1545	4	02	00	00
19951215	1234	6	02	00	00
19950619	1035	2	02	01	00
19950617	1155	7	02	00	00
19950615	1231	5	01	00	00
19950609	1400	6	02	00	00
19950513	1115	7	02	00	00
19950324	1613	6	02	00	00
19941120	0532	1	01	00	00
19941019	0039	4	02	00	00
19940823	1422	3	02	00	00
19940718	1205	2	02	00	00
19940425	1945	2	02	00	00
19940422	1145	6	02	00	00
19940212	1215	7	02	00	00
19940129	0808	7	01	00	00
19940108	1440	7	02	01	00
19931021	1125	5	02	00	00
19930720	1731	3	01	01	00
19930401	1324	5	02	00	00
19930319	1130	6	02	00	00
19930302	1610	3	02	00	00
19930212	1109	6	02	02	00
19921223	1400	4	02	00	00
19921207	0950	2	02	02	00
19921001	1646	5	02	00	00
19920902	1802	4	02	00	00
19920815	2021	7	02	01	00
19920607	1700	2	02	00	00
19920502	2300	7	01	00	00
19920422	1158	4	02	00	00
19920410	1345	6	02	00	00
19920205	1550	4	02	00	00
19920122	0040	4	02	00	00
19911217	1500	3	02	00	00

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestians
19911213	1215	6	02	00	00
19911213	1715	6	01	01	01
19911114	1538	5	02	00	00
19911110	1903	1	01	00	00
19910920	0750	6	02	01	00
19910918	1040	4	02	00	00
19910914	1330	7	02	00	00
19910629	1215	7	02	00	00
19910619	1820	4	02	00	00
19910609	2315	1	2	1	0
19910518	1235	7	02	00	00

Count: 58

MAIN STREET ACCIDENTS  
MAY 1991 TO JANUARY 28, 1997

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestians
19960926	1210	5	02	00	00
19950725	1410	3	02	00	00
19950710	1202	2	02	00	00
19940710	1240	1	01	00	00
19940704	2210	2	02	00	00
19940615	1521	4	02	00	00
19940430	0942	7	02	01	00
19940401	1500	6	02	01	00
19931214	1125	3	02	00	00
19930727	1051	3	02	00	00
19930714	1305	3	02	00	00
19930621	1110	2	02	00	00
19920720	1700	2	02	00	00
19920720	1431	2	01	00	00
19911214	1220	7	02	00	00
19911206	0433	6	02	00	00
19911121	1000	5	02	00	00
19911101	0855	6	01	01	01

Count: 18

# PHILLIPS STREET ACCIDENTS

MAY 1991 TO JANUARY 28, 1997

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestrians
19970106	1245	2	02	00	00
19961115	2359	6	01	00	00
19960726	1315	6	02	00	00
19960712	1816	6	02	00	00
19960615	1635	7	02	01	00
19960405	1037	6	02	00	00
19960317	1647	1	02	00	00
19960307	1507	5	01	01	00
19960215	1215	5	02	00	00
19960110	0948	4	01	00	00
19960107	1750	1	02	00	00
19960107	1822	1	01	00	00
19951209	1149	7	01	00	00
19951129	0635	4	01	00	00
19950912	1456	3	02	01	00
19950806	0832	1	01	01	00
19950714	1238	6	02	01	00
19950617	1305	7	02	01	00
19950507	2133	1	01	00	00
19950507	0731	1	01	00	00
19950501	1248	2	02	00	00
19950409	0016	1	01	01	00
19950406	1903	5	02	00	00
19950321	1523	3	02	00	00
19950222	0919	4	02	00	00
19950204	0915	7	01	00	00
19950204	1202	7	02	02	00
19950201	1431	4	02	00	00
19940908	1647	5	02	00	00
19940809	0910	3	02	01	00
19940615	1500	4	01	00	00
19940607	1015	3	02	00	00
19940604	1120	7	02	00	00
19940330	0117	4	01	02	00
19940111	1300	3	02	00	00
19931129	1216	2	02	00	00
19931127	1400	7	02	01	00
19931120	0950	7	02	00	00
19931119	0308	6	01	00	00
19931004	1345	2	02	00	00
19930927	1532	2	02	00	00
19930823	1753	1	02	00	00
19930620	1845	1	02	00	00
19930311	0802	5	02	03	00
19930306	1357	7	01	00	00
19930304	0158	5	01	00	00
19930212	1447	6	01	00	00

Date Occurred	Time	Day	Total Vehicles	Total Injured	Total Pedestians
19930212	1513	6	01	01	00
19930212	1514	6	02	01	00
19930109	1855	7	01	02	00
19930108	1112	6	02	02	00
19921212	0909	7	01	00	00
19921126	0130	5	02	00	00
19920716	1639	5	02	01	00
19920712	2216	1	01	04	00
19920528	1125	5	02	00	00
19920309	1040	2	2	0	0
19920201	1330	7	02	00	00
19920130	1521	5	02	00	00
19910721	0101	1	02	02	00

Count: 60

**WICKFORD VILLAGE**  
North Kingstown

THE WICKFORD VILLAGE PLAN ADVISORY COMMITTEE (APPOINTED BY THE TOWN COUNCIL) ASKS FOR YOUR HELP IN PREPARING A PLAN TO PROTECT THE SPECIAL QUALITY OF WICKFORD VILLAGE. PLEASE COMPLETE THE FOLLOWING QUESTIONNAIRE AND RETURN IT TO THE DEPARTMENT OF PLANNING AND DEVELOPMENT AT 55 BROWN STREET, WHERE YOUR RESPONSES WILL BE COMPILED.

Please check the response most appropriate to you: -

1. Are you : As a North Kingstown resident, do you depend more on:  
 a) A Wickford Resident \_\_\_\_\_ a) Wickford Merchants \_\_\_\_\_  
 b) A Wickford Merchant \_\_\_\_\_ b) Merchants outside Wickford \_\_\_\_\_  
 c) Employed in Wickford? \_\_\_\_\_ c) Both more or less equal \_\_\_\_\_  
 d) None of the above \_\_\_\_\_

2. How long have you lived in North Kingstown? \_\_\_\_\_

3. In your opinion, is Wickford Village a positive factor for choosing to live in North Kingstown?  
 Yes  No

4. Please list three things you like most about Wickford Village.

- a. \_\_\_\_\_  
 b. \_\_\_\_\_  
 c. \_\_\_\_\_

5. Generally for what purpose do you visit Wickford Village?  
 \_\_\_\_\_

6. As a North Kingstown resident how often do you visit Wickford Village in a week?  
 1 or 2 times \_\_\_\_\_ 3 to 5 times \_\_\_\_\_ More than 5 times \_\_\_\_\_

7. As a North Kingstown resident/merchant, do you think the historic character of Wickford should be protected and enhanced? Please comment.  
 Yes  No  \_\_\_\_\_

8. As a North Kingstown resident/merchant, do you support regulation required to maintain the historic character of Wickford? Please comment.  
 Yes  No  \_\_\_\_\_

9. Please rank three most important improvements you would like to see in Wickford Village.  
 (No. 1 as most important and No. 3 as least important).

Landscape \_\_\_\_\_ Parking \_\_\_\_\_ Crosswalks \_\_\_\_\_ Traffic control \_\_\_\_\_ Signage \_\_\_\_\_ Facades \_\_\_\_\_ Lighting \_\_\_\_\_  
 Waterfront walkways \_\_\_\_\_ Bicycle Access \_\_\_\_\_ Shop Diversity \_\_\_\_\_ Sidewalks \_\_\_\_\_ Other \_\_\_\_\_

10. Do you have a boat? Yes  No  Do you visit Wickford Village by boat? Yes  No   
 What additional facilities are needed in Wickford to support boating? \_\_\_\_\_

11. Please make other suggestions /comments about Wickford Village that you may want to share with us.

\_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

THE COMMITTEE WOULD LIKE TO THANK YOU FOR YOUR VALUABLE INPUT.

**WICKFORD VILLAGE PLAN  
SURVEY RESULTS**

**DRAFT REPORT**

**November 6, 1997**

WICKFORD VILLAGE  
North Kingstown

THE WICKFORD VILLAGE PLAN ADVISORY COMMITTEE (APPOINTED BY THE TOWN COUNCIL) ASKS FOR YOUR HELP IN PREPARING A PLAN TO PROTECT THE SPECIAL QUALITY OF WICKFORD VILLAGE. PLEASE COMPLETE THE FOLLOWING QUESTIONNAIRE AND RETURN IT TO THE DEPARTMENT OF PLANNING AND DEVELOPMENT AT 55 BROWN STREET, WHERE YOUR RESPONSES WILL BE COMPILED.

Please check the response most appropriate to you: -

1. Are you : As a North Kingstown resident, do you depend more on:
- a) A Wickford Resident 32.0% a) Wickford Merchants 13.5%
  - b) A Wickford Merchant 2.8% b) Merchants outside Wickford 30.9%
  - c) Employed in Wickford? 3.9% c) Both more or less equal 50.6%
  - d) None of the above 48.9%

A+B = 22% A+C = 3.9%  
2. How long have you lived in North Kingstown? 7 = 19.1 YRS

3. In your opinion, is Wickford Village a positive factor for choosing to live in North Kingstown?  
Yes  86.5% No  10.1%

4. Please list three things you like most about Wickford Village.

- a. \_\_\_\_\_
- b. SEE ATTACHED LIST
- c. \_\_\_\_\_

5. Generally for what purpose do you visit Wickford Village?

SEE ATTACHED LIST

6. As a North Kingstown resident how often do you visit Wickford Village in a week?  
1 or 2 times 37.1% to 5 times 36.0% More than 5 times 22.5%

7. As a North Kingstown resident/merchant, do you think the historic character of Wickford should be protected and enhanced? Please comment.

Yes  No  YES = 94.9% NO = 3.4%

8. As a North Kingstown resident/merchant, do you support regulation required to maintain the historic character of Wickford? Please comment.

Yes  No  YES = 89.3% NO = 7.9%

9. Please rank three most important improvements you would like to see in Wickford Village. (No.1 as most important and No.3 as least important). SEE ATTACHED PAGE

Landscape      Parking      Crosswalks      Traffic control      Signage      Facades      Lighting       
Waterfront walkways      Bicycle Access      Shop Diversity      Sidewalks      Other     

10. Do you have a boat? Yes  No  25.8% 73.6% Do you visit Wickford Village by boat? Yes  No  19.7% 78.7%  
What additional facilities are needed in Wickford to support boating? \_\_\_\_\_

11. Please make other suggestions /comments about Wickford Village that you may want to share with us.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

THE COMMITTEE WOULD LIKE TO THANK YOU FOR YOUR VALUABLE INPUT.

WICKFORD VILLAGE PLAN  
SURVEY RESPONSES

Response No. 4: Three things you like most about Wickford Village.

Top ten most frequent responses.

1. Nice shops, variety of services	65
2. Friendliness, sense of community	59
3. Quaintness, village charm	52
4. Historic character, architecture	49
5. Waterfront, harbor, water views	43
6. Ambience, beauty, atmosphere, aesthetics	31
7. Small New England town atmosphere	30
8. Pedestrian friendly, walking community	16
9. Functional village w/mix of activities	13
10. Clean, quiet, neat, not congested	13

Other responses include: Special seasonal activities & events, freedom of expression, accessibility, no chain stores, no traffic lights, wonderful place to bring visitors, Updike Park, Wilson Park, feeding the ducks, no barrooms, low crime, public safety, good police and fire departments, and the decorative arrangements on the poles.

Some responses noted the merchants' involvement in the village. Many respondents actually mentioned particular stores. Ryans received mention 18 times. Other stores with multiple responses were Wilson's, Earnshaws, the Diner, and the banks. The Library was mentioned 5 times. The Art Festival, summer concerts and the Festival of Lights were specifically mentioned as seasonal activities that respondents like. Several noted that there are no parking problems in the Village.

WICKFORD VILLAGE PLAN  
SURVEY RESPONSES

Response No. 5: For what purpose do you visit Wickford Village?

Top ten most frequent responses.

1. Shopping/services	146
2. Walking	38
3. Eating	18
4. Browse, sight see, recreation	18
5. Relaxation (i.e. feed ducks)	17
6. Attend church	9
7. Bring visitors, tour	8
8. Attend festivals, concerts	7
9. It's my home	7
10. Work	5
Socialize	5

## ALL RESPONSES

Category	Frequency and Rank			
	FIRST	SECOND	THIRD	OTHER
① Landscape	7.9%	10.7%	8.4%	0.6%
② Parking	17.4	9.6	9.6	1.1
③ Crosswalks	5.6	6.2	3.9	—
④ Traffic Control	21.3	12.9	5.6	1.1
⑤ Signage	2.2	1.7	2.2	0.6
⑥ Facades	5.6	4.5	5.1	—
⑦ Lighting	5.1	1.7	3.9	—
⑧ Waterfront Walkways	15.2	15.7	18.0	0.6
⑨ Bicycle Access	3.9	9.6	11.2	—
⑩ Shop Diversity	5.6	3.9	7.3	0.6
⑪ Sidewalks	24.2	11.8	7.3	—
⑫ Other	7.3	5.1	4.5	2.8

WICKFORD VILLAGE  
North Kingstown

THE WICKFORD VILLAGE PLAN ADVISORY COMMITTEE (APPOINTED BY THE TOWN COUNCIL) ASKS FOR YOUR HELP IN PREPARING A PLAN TO PROTECT THE SPECIAL QUALITY OF WICKFORD VILLAGE. PLEASE COMPLETE THE FOLLOWING QUESTIONNAIRE AND RETURN IT TO THE DEPARTMENT OF PLANNING AND DEVELOPMENT AT 55 BROWN STREET, WHERE YOUR RESPONSES WILL BE COMPILED.

Please check the response most appropriate to you: -

1. Are you : As a North Kingstown resident, do you depend more on:
- a) A Wickford Resident \_\_\_\_\_ a) Wickford Merchants 19.3%
- b) A Wickford Merchant \_\_\_\_\_ b) Merchants outside Wickford 19.3%
- c) Employed in Wickford? \_\_\_\_\_ c) Both more or less equal 54.4%
- d) None of the above \_\_\_\_\_

2. How long have you lived in North Kingstown? 9 = 17.9 YRS.

3. In your opinion, is Wickford Village a positive factor for choosing to live in North Kingstown?  
Yes  96.5% No  3.5%

4. Please list three things you like most about Wickford Village.

- a. \_\_\_\_\_
- b. DATA AVAILABLE FOR ALL RESIDENTS OF TOWN ONLY.
- c. \_\_\_\_\_

5. Generally for what purpose do you visit Wickford Village?

DATA AVAILABLE FOR ALL RESPONSES ONLY.

6. As a North Kingstown resident how often do you visit Wickford Village in a week?  
1 or 2 times 22.8% to 5 times 36.8% More than 5 times 35.1%

7. As a North Kingstown resident/merchant, do you think the historic character of Wickford should be protected and enhanced? Please comment.

Yes  No  YES = 98.2% NO = 1.8%

8. As a North Kingstown resident/merchant, do you support regulation required to maintain the historic character of Wickford? Please comment.

Yes  No  YES = 91.2% NO = 7.0%

9. Please rank three most important improvements you would like to see in Wickford Village. (No.1 as most important and No.3 as least important). SEE ATTACHED PAGE

Landscape \_\_\_\_\_ Parking \_\_\_\_\_ Crosswalks \_\_\_\_\_ Traffic control \_\_\_\_\_ Signage \_\_\_\_\_ Facades \_\_\_\_\_ Lighting \_\_\_\_\_  
Waterfront walkways \_\_\_\_\_ Bicycle Access \_\_\_\_\_ Shop Diversity \_\_\_\_\_ Sidewalks \_\_\_\_\_ Other \_\_\_\_\_

10. Do you have a boat? Yes  No  29.8% 68.4% Do you visit Wickford Village by boat? Yes  No  21.1% 75.4%  
What additional facilities are needed in Wickford to support boating? \_\_\_\_\_

11. Please make other suggestions /comments about Wickford Village that you may want to share with us.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

THE COMMITTEE WOULD LIKE TO THANK YOU FOR YOUR VALUABLE INPUT.

WICKFORD RESIDENTS

Category	Frequency and Rank			
	FIRST	SECOND	THIRD	OTHER
① Landscape	7.0%	14.0%	7.0%	—
② Parking	12.3	7.0	10.5	—
③ Crosswalks	8.8	7.0	3.5	—
④ Traffic Control	22.8	12.3	3.5	3.5
⑤ Signage	1.8	1.8	1.8	—
⑥ Facades	5.3	8.8	5.3	—
⑦ Lighting	5.3	—	—	—
⑧ Waterfront Walkways	8.8	12.3	19.3	1.8
⑨ Bicycle Access	5.3	5.3	14.0	—
⑩ Shop Diversity	5.3	7.0	1.8	—
⑪ Sidewalks	24.6	12.3	7.0	—
⑫ Other	10.5	8.8	1.8	1.8

NON - WICKFORD RESIDENTS

WICKFORD VILLAGE  
North Kingstown

THE WICKFORD VILLAGE PLAN ADVISORY COMMITTEE (APPOINTED BY THE TOWN COUNCIL) ASKS FOR YOUR HELP IN PREPARING A PLAN TO PROTECT THE SPECIAL QUALITY OF WICKFORD VILLAGE. PLEASE COMPLETE THE FOLLOWING QUESTIONNAIRE AND RETURN IT TO THE DEPARTMENT OF PLANNING AND DEVELOPMENT AT 55 BROWN STREET, WHERE YOUR RESPONSES WILL BE COMPILED.

Please check the response most appropriate to you: -

1. Are you : As a North Kingstown resident, do you depend more on:
- a) A Wickford Resident \_\_\_\_\_ a) Wickford Merchants 6.9%
  - b) A Wickford Merchant \_\_\_\_\_ b) Merchants outside Wickford 41.4%
  - c) Employed in Wickford? \_\_\_\_\_ c) Both more or less equal 48.3%
  - d) None of the above \_\_\_\_\_

2. How long have you lived in North Kingstown? 7 = 19.01 yrs.

3. In your opinion, is Wickford Village a positive factor for choosing to live in North Kingstown?  
Yes  80.5% No  13.8%

4. Please list three things you like most about Wickford Village.

- a. \_\_\_\_\_
- b. DATA AVAILABLE FOR ALL RESIDENTS OF TOWN ONLY.
- c. \_\_\_\_\_

5. Generally for what purpose do you visit Wickford Village?  
DATA AVAILABLE FOR ALL RESPINSES ONLY.

6. As a North Kingstown resident how often do you visit Wickford Village in a week?  
1 or 2 times 54.3% to 5 times 35.6% More than 5 times 5.7%

7. As a North Kingstown resident/merchant, do you think the historic character of Wickford should be protected and enhanced? Please comment.  
Yes  No  YES = 92.0% NO = 4.6%

8. As a North Kingstown resident/merchant, do you support regulation required to maintain the historic character of Wickford? Please comment.  
Yes  No  YES = 86.2% NO = 9.2%

9. Please rank three most important improvements you would like to see in Wickford Village. (No.1 as most important and No.3 as least important). SEE ATTACHED PAGE  
Landscape \_\_\_\_\_ Parking \_\_\_\_\_ Crosswalks \_\_\_\_\_ Traffic control \_\_\_\_\_ Signage \_\_\_\_\_ Facades \_\_\_\_\_ Lighting \_\_\_\_\_  
Waterfront walkways \_\_\_\_\_ Bicycle Access \_\_\_\_\_ Shop Diversity \_\_\_\_\_ Sidewalks \_\_\_\_\_ Other \_\_\_\_\_

10. Do you have a boat? Yes  No  16.1% 83.9% Do you visit Wickford Village by boat? Yes  No  12.6% 86.2%  
What additional facilities are needed in Wickford to support boating? \_\_\_\_\_

11. Please make other suggestions /comments about Wickford Village that you may want to share with us.  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

## NON - WICKFORD RESIDENTS

Category	Frequency and Rank			
	FIRST	SECOND	THIRD	OTHER
① Landscape	8.0 %	9.2 %	10.3 %	1.1 %
② Parking	20.7	8.0	9.2	1.1
③ Crosswalks	4.6	6.9	5.7	—
④ Traffic Control	19.5	12.6	8.0	—
⑤ Signage	1.1	2.3	1.1	—
⑥ Facades	5.7	1.1	5.7	—
⑦ Lighting	5.7	3.4	5.7	—
⑧ Waterfront Walkways	19.5	23.0	16.1	—
⑨ Bicycle Access	4.6	11.5	9.2	—
⑩ Shop Diversity	5.7	5.7	9.2	—
⑪ Sidewalks	20.7	12.6	8.0	—
⑫ Other	4.6	2.3	6.9	—



# Town of North Kingstown

Rhode Island

No. 19

## TOWN COUNCIL

David R. Burnham

Council President

Robert E. Craven

Council Member

John V. Gibbons

Council Member

Betty D. Givan

Council Member

George W. Graham

Council Member

November 18, 1996

At the Regular Meeting of the Town Council of the Town of North Kingstown

held on November 18, 1996, a resolution was passed as follows:

WHEREAS, the Town Council desires to continue to implement the goals and objectives of the Comprehensive Plan of the Town of North Kingstown, as adopted August 17, 1992; and

WHEREAS, the Vision Statement of the Land Use Element of the Comprehensive Plan seeks to protect the traditional New England character of North Kingstown by preserving historic areas and maintaining aesthetic values; and

WHEREAS, the Comprehensive Plan recommends a thorough analysis of each village to determine its unique characteristics and qualities, as well as its historic, natural, and cultural resources; to ensure that village character is preserved; and to ensure that street furniture, lighting, and signage are compatible with local character; and

WHEREAS, Wickford Village, being the oldest village in the Town of North Kingstown, is on the National Register of Historic Places and is in the Town's only local Historic District; and

WHEREAS, Wickford Village continues to maintain its historic place as a residential community and a center of cultural and economic activities; and

WHEREAS, the National Trust for Historic Preservation has awarded the Town of North Kingstown a matching grant from the Antoinette F.-Downing Preservation Services Fund for the preparation of the Wickford Village Plan.

NOW, THEREFORE, BE IT RESOLVED: that it is the responsibility of the Wickford Village Plan Advisory Committee to direct the preparation of a Plan for Wickford Village to preserve and enhance the traditional character of Wickford Village and its economic vitality by developing guidelines for public improvements; and by developing design standards for lighting, utilities, parking, street trees, pedestrian movement, vehicular circulation, and access to local businesses, the waterfront, and to public spaces.

  
James D. Marques, CMC  
Town Clerk

WICKFORD VILLAGE PLAN  
SUBCOMMITTEE MEMBERSHIP LIST  
REVISED MAY 12, 1997

WATERFRONT/MARITIME

Malcolm Long (resigned effective 5/12/97)

Jesse Nalle (appointed 5/12/97)

Empie Davis

Les Stevens

Al Lewis

VILLAGE INFRASTRUCTURE

Ralph Rizzo

Frank Romeo

Jim Wynne

Bob Fitzsimmons

Paul Wilson

Peter Morgan

PROTECTING HISTORIC CHARACTER/  
GROWTH MANAGEMENT

Harry Beckwith

Kathy Romeo

irving Spaulding

Robin Porter

Jack Renshaw

Maureen Johnson (appointed 5/12/97)

Carol Gibson

Kurt van Dexter

Mary Turkel

Marsue Harris

Unassigned

Ed Lennon

Ernest Moncrief (new member appointed 5/12/97)

Katharine Cutting (new member appointed 5/12/97)



# Town of North Kingstown

Rhode Island

No. 18

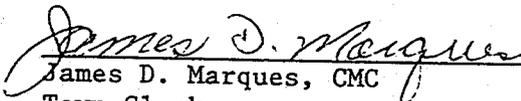
## TOWN COUNCIL

David R. Burnham  
*Council President*  
Kenneth C. D'Ambrosio  
*Council Member*  
Betty D. Givan  
*Council Member*  
George W. Graham  
*Council Member*  
Dale M. Grogan  
*Council Member*

May 12, 1997

At the Regular Meeting of the Town Council of the Town of North Kingstown held on May 12, 1997, it was

VOTED: To accept the letter of resignation from Malcolm C. Long, resigning from the Wickford Village Plan Advisory Committee, and that a letter of appreciation be sent to him for the dedicated service he gave to the Town of North Kingstown.

  
James D. Marques, CMC  
Town Clerk



# Town of North Kingstown

Rhode Island

No. 20

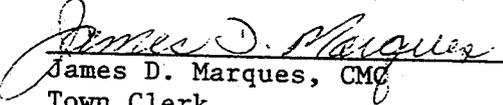
## TOWN COUNCIL

David R. Burnham  
*Council President*  
Kenneth C. D'Ambrosio  
*Council Member*  
Betty D. Givan  
*Council Member*  
George W. Graham  
*Council Member*  
Dale M. Grogan  
*Council Member*

May 12, 1997

At the Regular Meeting of the Town Council of the Town of North Kingstown held on May 12, 1997, it was

VOTED: To appoint Ernest Moncrieff, 11 Fountain Street, to the Wickford Village Plan Advisory Committee to December 31, 1997, to fill the unexpired term of Malcolm C. Long.

  
James D. Marques, CMC  
Town Clerk



# Town of North Kingstown

Rhode Island

No. 21

## TOWN COUNCIL

David R. Burnham  
Council President  
Kenneth C. D'Ambrosio  
Council Member  
Betty D. Givan  
Council Member  
George W. Graham  
Council Member  
Dale M. Grogan  
Council Member

May 12, 1997

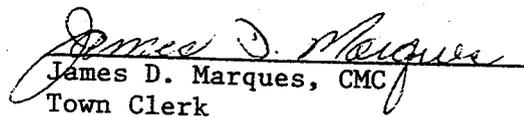
At the Regular Meeting of the Town Council of the Town of North Kingstown held on May 12, 1997, it was

VOTED: That the Wickford Village Plan Advisory Committee membership be increased from sixteen (16) to nineteen (19) members, and that the following three (3) members be appointed to December 31, 1997:

Katharine Cutting, 465 Boston Neck Road  
Mailing: PO Box 1067, Davisville, 02854

Maureen Johnson, 32 Stillman Road, Saunderstown  
Mailing: PO Box 424, Saunderstown

Jesse Nalle, 66 Willett Road, Saunderstown

  
James D. Marques, CMC  
Town Clerk